

**Ward:** Bury East - Redvales

Item 01

**Applicant:** East Lancashire Light Railway Co. Ltd

**Location:** Buckley Wells Locomotive Depot, Baron Street, Bury, BL9 0TY

**Proposal:** Development of the ELR's historic locomotive and carriage works site including repair and restoration of the 1857 locomotive work building, the construction of new locomotive running shed, incorporation of associated new workshops and training facilities and construction of a standalone carriage shed.

**Application Ref:** 66745/Full

**Target Date:** 21/06/2021

**Recommendation:** Approve with Conditions

**The Development Manager has recommended a site visit prior to the Planning Control Committee meeting.**

### **Description**

The application relates to Buckley Wells which is the location and facility of the locomotive and carriage works and rail depot for the East Lancashire Railway (ELR).

The site is located on Baron Street which is formed off Manchester Old Road. The eastern part of Baron Street is adopted and comprises residential properties on either side of the street. There is an apartment block located next to the site where Baron Street then becomes a part tarmac part cobbled unadopted road, classified as a By-Way Open to All Traffic. At this point the main entrance to the site is formed. Baron Street then continues westwards and leads to 3 separate schools (St Gabriel's, Bury Girls and Boys Grammar School).

The overall Buckley Wells site area is 9.1ha. The site is split into two parts. The main operational part of the site is 5.5ha and is immediately to the south of Baron Street and comprises a Grade II Listed locomotive works/carriage shed building, an extensive network of railway sidings, storage containers, open air storage for a wide range of railway infrastructure and engineering components and parking areas.

The southern part of the site has a narrow elongated triangular form, with Baron Street forming the northern boundary, the eastern boundary is defined by the Metrolink line which is screened from the site by dense planting and to the west is a steep embankment. It is within a Wildlife Corridor and partly within a River Valley.

This part of the site is the subject of this planning application.

The other area of the Buckley Wells site is located opposite, to the north of Baron Street which is additional operational land comprises a 'modern' carriage shed building constructed in the 1980's and further railway sidings. To the west is an area of un-used land set at a lower level to the main site which is overgrown with self-seeded vegetation.

### **Background to the application**

The ELR plays a key role as a visitor and heritage attraction in the north west and has its main workshop facilities located at the Buckley Wells site where the locomotives, carriages and wagons are housed and where the essential locomotive and carriage operations, maintenance and restoration facilities necessary to operate the successful heritage railway are carried out.

In recent years the ELR Company has been pursuing a scheme of works to help improve visitor experience and Buckley Wells, and as the centre for the railway operations, Buckley Wells is the critical and indispensable base for the storage and maintenance of historic

locomotives and carriages such as the Flying Scotsman.

The works are currently carried out in two buildings -

- A Grade II Listed carriage shed building dating from 1857 which is located immediately to the south of Baron Street. This houses the engineering facilities for the maintenance and restoration of historic locomotives and facilities for both long term restoration projects and operational locomotives in daily use. This building is deteriorating badly and in need of significant investment to secure its longevity not only as a valuable heritage asset but as vital facility for ELR and the site.
- A more modern carriage shed to the north of Baron Street which provides limited facilities for restoration works and storage.

Not only is there a need to carry out essential works to the Listed Building but there is also a need to provide additional purpose built provision for the fleet of locomotives and carriages so they can be maintained to the highest possible standards expected of such an established and high quality attraction and to secure the functions and capabilities to serve the ELR.

There is also an aspiration to expand the important and essential training facilities within the site to attract and train up future generations of volunteer engineers necessary and crucial to maintain and successfully operate the heritage railway for years to come.

In essence, development works are essential to facilitate the managed growth of the ELR on a long term and sustainable basis as a premier visitor and heritage attraction.

### **The proposal**

There are 5 key elements to the redevelopment of the Buckley Wells site -

1. A comprehensive programme of restoration and repair to the Grade II Listed historic Buckley Wells carriage shed building. It will continue to be used for the restoration of locomotives/general railway engineering operations including workshops and stores. An extension of 626 sqm to provide new offices would be located at the north end of the building to support training, skills and various operations associated with the overall site.
2. An extension to the LB carriage shed to provide a new locomotive running shed and maintenance workshop which would be located on the western side of the building and on the footprint of a former original Buckley Wells engine shed (demolished in the 1970's). This new build would have a portal frame structure with steel clad elevations which would also accommodate classroom/workshop areas, stores, and other ancillary facilities.
3. The erection of a new carriage shed which would house, restore and repair the ELR's fleet of historic railway coaches. This would be located on an area of sidings to the south of the Listed Building, approx 90m away. The building would be vital to the storing of the machines and engines and for the protection from the elements and vandalism, particularly as their maintenance can take an extensive period of time. The building would be formed by a base of red engineering brick with steel clad elevations interspersed with a row of symmetrically aligned window openings. Solar panels would be located on the roof. A new internal access road would be formed to this building.
4. Within the site some tracks would be re-laid to facilitate rail access to the new buildings.
5. It is also proposed to rationalise the external part of the site to provide for the expanded operational capacity of the site and would include new areas of hardstanding, designated parking areas for staff and volunteers and external storage facilities in the eastern part of the site.

The scheme is proposed as a phased development comprising 3 components -

Phase 1 - Lifting and relaying rail track to establish facilitate and new track layout and rationalise the areas for storage around the site. The site would also be cleared for the construction works for the new build.

Phase 2 - Construction of the new build carriage shed and vehicular access route to the building.

Phase 3 - Works to restore the Listed carriage shed and construction of the proposed

running shed.

### **Relevant Planning History**

47480 - Workshop extension to carriage and wagon shed - Approve with Conditions 28/02/2007

49349 - Carrying out of an engineered fill operation prior to accommodating new rail track infrastructure without the restriction within condition 10 of planning permission ref. 48268 that would limit the hours during which the importation of materials to carry out the operation can take place. - Approve with Conditions 19/03/2008

49958 - Erection of 2.4 metre high paladin security fencing from the Metrolink boundary along the length of the bury grammar schools playing fields access road boundary, and returning along baron street to the east Lancashire railway level crossing (retrospective) - Approve 19/09/2008

53647 - Renewal of planning permission 49349 to carry out engineering fill operation prior to accommodating new rail track infrastructure - Approve with Conditions 20/04/2011

### **Publicity**

Letters sent to 122 properties on 30/3/21.

Site notice posted 15/4/21

Press advert 1/4/21

No representations made by the public have been received.

### **Statutory/Non-Statutory Consultations**

**Traffic Section** - No objection. Conditions to be recommended in the Supplementary Planning Agenda.

**Borough Engineer - Drainage Section** - No response received

**Environmental Health - Contaminated Land** - No objection subject to condition

**Environmental Health - Pollution Control** - No objection

**Environmental Health - Commercial Section** - No response received

**Public Rights of Way Officer** - No objection

**Waste Management** - No response received

**United Utilities (Water and waste)** - Recommend a condition for a detailed water drainage scheme.

**Transport for Greater Manchester (TfGM)** - No objection in principle

**TfGM Metrolink** - Require a legal agreement regarding the use of Metrolink land

**Rossendale Borough Council** - Objection received - see report below for a summary and response

**The Coal Authority** - No objection

**Planning & Building Regs consultation Fire Protection Dept Bury Fire Station (Part B)**

- Advisory note to applicant

**Greater Manchester Ecology Unit** - No objection subject to conditions and informatives

**Pre-start Conditions** - Applicant/Agent has agreed with pre-start conditions

### **Unitary Development Plan and Policies**

NPPF National Planning Policy Framework

EC1/2 Land Suitable for Business (B1)

EN1/2 Townscape and Built Design

EN2/3 Listed Buildings

EN6/4 Wildlife Links and Corridors

EN7 Pollution Control

EN7/1 Atmospheric Pollution

OL5/2 Development in River Valleys

HT2/4 Car Parking and New Development

HT3/4 Schemes to Assist Metrolink

HT4 New Development

HT6/2 Pedestrian/Vehicular Conflict

RT4/6 East Lancashire Railway

## **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

## **Policies and Principles**

### **Opportunity Site**

The majority of the site is allocated as an Opportunity Site in the Bury Unitary Development Plan suitable for proposals for the further development and enhancement of facilities associated with the East Lancashire Railway (RT4/6/5) or for Business (B1) and Office use (EC1/2/8).

UDP Policy RT4/6 relates to the East Lancashire Railway and states that Council will encourage and support proposals for the further development and enhancement of facilities associated with the East Lancashire Railway. In accordance with this policy, a number of sites are allocated for railway-related development, including the application site at Buckley Wells under allocation RT4/6/5. The proposal is consistent with Policy RT4/6 and the specific allocation under RT4/6/5.

The Opportunity Site allocation also identifies the site as being suitable for Business (B1) and Office Use under UDP Policy EC1/2 and, specifically, under proposal EC1/2/8. However, this opportunity reflected the site's proximity to a proposed new Park and Ride Metrolink station at Buckley Wells (Highways and Transportation Proposal HT3/4/1) which was considered to offer a unique opportunity to integrate a suitable employment use and a major new public transport facility. However, the proposal for a Park and Ride Metrolink stop is no longer active

Policy RT4/6/5 stresses the importance that further development of the railway is facilitated to maximise the number of visitors to both the railway and wider Irwell Valley, and positively encourages development aimed at safeguarding, improving or developing this important resource and associated proposals.

The ELR is a key visitor and heritage attraction, and the site carries out essential maintenance and operational related works necessary for the successful delivery of its facility and services. The proposed development would contribute and improve on the essential facilities to manage the expansion and enhancement programme which would secure and facilitate the ELR as a major visitor and heritage attraction for the long term future.

As such the proposals in principle are consistent with the currently adopted UDP policies on Recreation and tourism.

### **River Valley**

Part of the application site falls within an area that is designated as River Valley. UDP Policy OL5/2 relates to development in River Valleys and states that within the River Valleys, new buildings or the change of use of existing buildings or the change of use of land will not be permitted. The only exceptions considered acceptable will



be those where the development would not lead to the division of the open parts of the valleys into sections and it falls within the terms below:

- where the area is designated as Green Belt the established Green Belt policies will apply; or
- where the area does not form part of the Green Belt, at least one of the following circumstances is met:
- that the development represents limited infilling to an established valley settlement or industrial area;
- that it is an extension to, or renewal of an existing industry, where the economic and employment factors are of overriding importance;
- that the development is required in association with an outdoor recreation or appropriate tourist facility;
- that the development is limited and will form part of, and be essential to, the maintenance of the provision and improvement of public services and utilities;
- any other development that would be appropriate in a Green Belt.

As the land is not designated as Green Belt, it is necessary for the proposal to meet at least one of the exceptions set out in Policy OL5/2 above.

The proposals for the new locomotive running shed and the store compound would extend into the River Valley area. In this respect, the proposed development would be contained within the existing boundary of the Buckley Wells site on already developed land, and therefore encroach no further into the River Valley than currently exists and therefore would not lead to the division of open parts of the valley into sections. In addition, the proposal would involve an extension to an existing industry, where the economic and employment factors are of overriding importance as well as the development being required in association with an outdoor recreation or appropriate tourist facility.

Consequently, the proposal is considered not to conflict with UDP Policy OL5/2.

### **Heritage considerations**

The existing carriage shed at Buckley Wells is a Grade II Listed Building.

Planning (Listed Buildings and Conservation Areas Act) 1990 Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 require decision makers to have special regard to the desirability of preserving a listed building or its setting or any feature of architectural or historic interest it possesses. The decision maker must also give considerable importance and weight to the desirability of preserving the setting of the listed building.

Under UDP Policy EN2/3, the Council will actively safeguard the character and setting of Listed Buildings by not permitting works, alterations or changes of use which would have a detrimental effect on their historical or architectural character and features. Proposals for demolition will be opposed and will only be considered where it is demonstrated conclusively that the building(s) cannot be retained. Consideration of any potential impact on the Listed Building should be given to ensure that there is no conflict with UDP Policy EN2/3.

In terms of the NPPF, under para 194, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance.

195. Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

197. In determining applications, local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness.

199. When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

200. Any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification. Substantial harm to or loss of:

- a) grade II listed buildings, or grade II registered parks or gardens, should be exceptional;
- b) assets of the highest significance, notably scheduled monuments, protected wreck sites, registered battlefields, grade I and II\* listed buildings, grade I and II\* registered parks and gardens, and World Heritage Sites, should be wholly exceptional.

201. Where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- a) the nature of the heritage asset prevents all reasonable uses of the site; and
- b) no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation; and
- c) conservation by grant-funding or some form of not for profit, charitable or public ownership is demonstrably not possible; and d) the harm or loss is outweighed by the benefit of bringing the site back into use.

202. Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

The NPPF at Annex 2 describes setting as:

Setting of a heritage asset: The surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral;

and

Significance (for heritage policy): The value of a heritage asset to this and future generations because of its heritage interest. The interest may be archaeological, architectural, artistic or historic. Significance derives not only from a heritage asset's physical presence, but also from its setting. For World Heritage Sites, the cultural value described within each site's Statement of Outstanding Universal Value forms part of its significance.

An appropriate Heritage assessment has been submitted with the application in consideration of the above heritage paras of the NPPF.

### **Works to the Listed Building**

The proposal comprises of the extension of the existing 1857 building on its west elevation which would replace the previous extension built in 1875 and demolished in the 1970s. This would comprise of a two bay 4 road running shed. A steel portal construction it would have a red brick plinth and steel cladding system. The roof would be hipped with a

ventilation louvre along its length with solar panels and a translucent rooflight. In design terms the proposed extension draws inspiration from the original extension in terms of its size and scale with an acknowledgement to the architectural style of the original extension with its 2 bay construction and ventilation louvre. In terms of height the proposal would not dominate the existing shed and would not create unacceptable massing. Overall, the proposed extension does not attempt to be a pastiche of the original shed and as such it does not challenge the architectural aesthetic of the listed building or confuse its legibility as an historic building of merit and interest.

The proposed extensions to the 1862 offices at the north elevation of the shed also draws inspiration from the existing structure by virtue of its materials and height. This element, although part of the listing entry does not possess any special interest. The proposed office extension would provide much needed welfare, office and training facilities.

works/repairs - to fabric subject to LBC the

A comprehensive structural and condition report with a schedule of repair/restoration has been submitted as part of the listed building consent application. Careful consideration will be given to the amount, scale and scope and level of intervention of the required repairs/restoration. As a rule, repairs will be kept to the required minimum in order to preserve the significance of the building and this will be balanced against the need to preserve the fabric of the building with regard to air and water tightness and structural integrity.

The repairs and restoration are acceptable and comprise of external roof repairs, window repairs, external brick repairs and internal roof timber repairs.

### **Setting**

The proposed stand-alone carriage restoration workshop, carriage/diner and catering accommodation shed would be sited to the south west of the 1857 shed and would be within its setting. Its design philosophy is the same as that in approach to the design of the 1857 shed extension. It would be a portal frame 2 bay construction with brick plinth and a steel cladding system, ventilation louvre, solar panels. Its height at approximately 16 metres would be approximately 4 metres higher than the highest part of the original 1857 shed. This would have an impact on its setting.

The setting of a listed building are the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset and may affect the ability to appreciate that significance or may be neutral.

It is important to note that setting is not a land use or a heritage value. Development within or change to a setting can impact on the significance of the listed building. Buckley Wells is an industrial site by its nature, its setting has changed over its lifetime with the addition and removal of extensions to the original shed as well as addition and removal of permanent and semi-permanent outbuildings. The site currently has a number of cabins and carriages used as office space and for welfare and other ad hoc storage facilities within the site and adjacent to the east elevation of the shed which have a negative impact on the setting of the listed building. It is understood that proposals would allow these elements to be removed. This would have a positive impact on its setting.

### **Heritage Balance and Conclusion -**

Paragraph 201 of the NPPF states that where proposals amount to substantial harm to or total loss of significance to a heritage asset the application should be refused. It is not considered the proposals amount to this level of harm. The proposed extension to the shed and office will not require any significant loss of historic fabric of the shed or alter its use or adapt it in any way that would cause loss of significance as an architecturally important railway building or the continuing use of the site as a railway depot. The proposed extension would replace one that previously existed historically and as such together with the office extension would represent a new addition to the building. This would represent less than substantial harm within the terms of paragraph 202 of the NPPF. As such

paragraph 202 of the NPPF requires that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

It is considered the proposed extensions would bring considerable public benefits in protecting the long term viable use of the shed and site as heritage railway and which would also be compatible with its conservation. It would facilitate the expansion of the heritage railway in a viable and compatible way and using green energy securing its future as well as bringing economic and educational benefits. As such the proposed extensions would outweigh, by virtue of their public benefits, the less than substantial harm identified and therefore would comply with paragraph 202 of the NPPF.

The proposed stand-alone carriage restoration workshop, carriage/diner and catering accommodation shed would be sited to the south west of the 1857 listed shed and would be within its setting. The size and proximity of the proposed building together with associated works comprising of additional hardstanding and track layout would have an impact on the setting of the listed 1857 shed. The question is whether or not the proposal would alter the setting in such a way as to cause harm to the significance of the listed shed.

The site is an evolved and evolving industrial site. As a working railway depot, it is viewed in close by and wider distances within its industrial site context. The proposed stand-alone carriage shed would not alter this fact. However, the way in which the listed shed is experienced would be altered by the proximity of the proposal but given the context of the site this alteration would not lead to substantial harm to or total loss of significance with regard to paragraph 201 of the NPPF. Therefore, the proposed stand-alone carriage shed would represent less than substantial harm to the significance of the listed shed. This harm would be outweighed by the considerable public benefits brought by the scheme as already identified above.

The proposal would therefore preserve the identified special historic and architectural significance of the grade II listed Buckley Wells locomotive and carriage shed and its setting. The proposed restoration works and extension to the Listed Building would ensure the longevity of the building and preserve the building and its use for years to come. It is therefore considered the proposed development would be acceptable in principle and comply with the principles of the NPPF and policy EN2/3.

The works which involve the Listed building and impact on the setting on the Listed Building are assessed more fully in the parallel application ref 67864 which is the associated Listed Building Consent application.

Conditions should be imposed on both the planning and associated LBC applications for the formal assessment of condition and method of repair, scope and intervention to be submitted to and approved in writing by the local planning authority before the commencement of works for repair or restoration.

It is recommended that a condition is also imposed for the submission of details of the external finishes for the proposed extension and stand-alone carriage building to be submitted to and approved in writing by the local planning authority before any development commences.

### **Layout and siting**

Site constraints including the configuration of the track layout, form and shape of the overall site, accesses to the site and the position of the Listed Building have informed the development proposals.

Proposed new carriage shed - By its very nature, a purpose built carriage restoration workshop needs to be large and elongated in form to not only enable the necessary works, restorations, maintenance and repairs to take place but a facility to also accommodate the

historic carriages which are currently stored outside and are vulnerable to inclement weather and vandalism attacks. Facilitating their safe and secure storage would also double the life-span of the carriages to 10 years.

Given the site constraints and necessary requirements, it is therefore proposed to site the carriage shed in the southern part of the site which is the only area capable of accommodating the 6065 sqm building and one which would also relate well in terms of legibility and connections to the Listed Building and wider site.

To access the building, it is proposed to lay a new internal vehicular route from the car park which would run along the eastern boundary of the site. This would not interfere with other built development on site and also avoid the existing rail tracks. There would also be an area to the west of the building which would be capable of providing a servicing and car park for deliveries, staff and visitors without impacting on the wider site.

Internally, the building would provide areas for the storage of the carriages, separate to those being maintained and repaired. The main staff facilities would also be located within the building together with the catering block for the food preparations for the Red Rose Pullman Diner which is a popular tourist attraction.

It is therefore considered the siting and layout of the proposed carriage shed could be accommodated in this part of the site without affecting the operations of the Buckley Wells site or adversely impacting on the River Valley.

Existing Listed Building - Proposed administration and training block - The main site administrative office space, staff/volunteer mess and welfare facilities, and meeting rooms are currently provided by the two temporary buildings located on the east side of the Listed Building. These temporary buildings are unsightly, inadequate for the current and future level of activity planned for the site and are becoming in an increasingly dilapidated condition.

The proposal is to replace and expand these facilities by adapting and extending the brick-built office block that sits on the Baron Street end of the Listed Building.

It is proposed to demolish the small courtyard and toilet outhouse that sits on the north east corner of the Listed Building and to construct an extension to the office block in the space left by the demolition. The ground floor of the extension would provide a reception area that would act as the main control point for managing access into the Running Shed/Heavy Maintenance Workshop, and into the site overall. The first floor of the extension would contribute to providing the meeting room/training room facilities.

The west side of the LB would also be extended. The ground floor of the proposed extension would provide the space for the welfare facilities and the first floor would provide a large mess room serving the entire building complex.

Facilitating this accommodation within the footprint and confines of the Listed Building would not only provide much needed and upgraded facilities but would locate the functions within one area to streamline and rationalise the operations of the ELR.

Proposed new Running Shed - The purpose of the proposed Running Shed would be to significantly ease the pressure on the Listed Building which has reached its maximum capacity and which negatively impacts on working conditions and also limits the number of locomotives that can be worked on at one time. Such intensification of the use of the building is also putting a physical strain on its historic fabric. Providing a new building would relieve the pressure on the LB, enable a more spacious and efficient working environment to be achieved and facilitate the equipment and machinery necessary to deliver the ELR operations.

Ideally, the two builds need to be physically connected and site constraints including the alignment of the rail tracks within the site and position of the existing access from Baron

Street have been determining factors to where and how the building would be positioned.

It is therefore proposed to position the new build on the western elevation of the Listed Building which previously accommodated an engine shed which was demolished in the 1970's and therefore an ideal position for a new build. This would allow connectivity to the LB by an existing door opening on the west elevation with an additional connection directly from the heavy workshop to facilitate the necessary operations which are carried out internally. The Running Shed would provide a heavy maintenance shop for the major locomotive restoration. The building would also provide accommodation for important support facilities - workshops, stores and a dedicated training workshop and welfare facilities.

Access from Baron Street would also be maintained and the new build able to connect via tracks to the new Carriage Shed which is proposed to the south of the site.

It is therefore considered the layout and siting of the proposed Running Shed would be best placed and positioned in relation to the functions and manageability of the ELR operations.

Track layout - It is proposed to make as much use of the existing tracks as possible but works would be required to move/add to some of the track to facilitate train access to the new building. It is noted that the existing lines leading to the LB carriage shed would remain unaltered.

Site storage - There are a number of shipping containers on the site, generally formed along the eastern boundary but in a slightly haphazard arrangement. It is proposed to re-configure the arrangement of the containers along the eastern boundary which would tidy up and streamline this area of the site.

It is also proposed to provide a separate storage compound for the associated materials and equipment which would further rationalise the site and better the overall arrangement.

Car park - This would be reconfigured to provide a formal parking layout in the eastern part of the site close to the entrance, thereby improving parking provisions for staff and visitors.

In terms of access to the site, the existing entrance at the north east corner of Baron Street would be the main access point for staff, volunteers, visitors and the smaller deliveries. There is also an entrance from Baron Street at the north westerly corner of the site with the proposal to utilise this access for the delivery of the locomotives and carriages by ELR low-loaders. This would require some improvement works and the setting back of gates opposite Baron Street which form the entrance to the northern part of the Buckley Wells site, which would enable such vehicles to manoeuvre the large loads.

In conclusion, it is considered that the proposed layout of the new buildings and extensions to the existing LB, together with the provision of the re-configured accesses, car parks and additional servicing/storage areas would provide a betterment of the existing arrangements, enabling the site to function in a productive and sustainable way which would be of benefit to all users, visitors and employees and in turn improve the service and attractions the ELR provides.

The proposals therefore accord with the principles of the NPPF and UDP policies.

**Design** - The design concept for the development has been driven by a number of factors including the need for the buildings to perform in a functional and practical fashion, awareness to the sensitivities of the Listed Building, whilst being challenged by an ambitious, potentially costly, but necessary scheme.

Turning to the works to the Listed Building, a scheme of restoration and repair is proposed, including the complete overhaul and repair of the original windows, strip out of the roof to incorporate a new breathable roofing felt re-using as many of the existing slates as possible, replacing valley gutters and flashing and install new glazed roof lights. Internally, repairs

and cleaning would be carried out to the walls, replacing roof trusses where necessary.

In terms of the extension to create office and welfare facilities to the front of the listed building on Baron Street, it is proposed to form the front elevation with red brick to half way with the remainder formed by zinc seam cladding. Bearing in mind the functionalities of the building as industrial uses, and its position on Baron Street which is more susceptible to street, this approach is considered acceptable maintaining appropriate definition between new and old.

These works are considered in more detail in the application for Listed Building Consent, but essentially it is concluded the fabric of the historic building would be sensitively treated and restoration would ensure the longevity of the building for years to come.

The challenge comes in the addition to the western elevation of the heavy maintenance workshop, as it is critical the new addition has a harmonious relationship to the LB in terms of form, massing and design whilst fulfilling its necessary functions. Instead of trying to replicate the LB which could result in a pastiche and poor imitation, the approach is one of a modern and contemporary addition which would stand on its own merits and distinguish it from the LB rather than vie for attention.

It is proposed to construct a portal framed build with a red brick plinth and steel built up cladded elevations with intervening flat cladded panels on the north west elevation. The roof would be hipped and able to support the mechanical extract plant which would be a key part of the roof design, providing a modern equivalence to ventilation lanterns and smokestacks found on more historic types of buildings.

Adding a modern style of build to a historic listed building is a common approach and in respect of this scheme, with the identified constraints and deliverability issues, considered to be an acceptable solution.

The design of the new Carriage Shed has similarly been driven by its function, size and height requirements to accommodate machinery and locomotives/carriages, together with the practicalities and costs of erecting the building. The design philosophy is the same as that in the approach to the 1857 shed extension. The design therefore proposes a portal frame profile metal clad industrial 'shed'. The use of cladding on the elevations would express the functionality of the build as a large scale industrial use and the use of large span portals would be a modern approach but equivalent in form and reflective of more historic railway buildings, form following function.

Translucent roof lights and solar panels would feature into the design, allowing natural light and energy saving mechanisms to be incorporated within the building without undue attention being drawn to them.

It is therefore considered the proposed new carriage shed would sit comfortably within the existing site in terms of its design and appearance and be a coherent and logical solution to providing the essential facilities at the ELR site.

It is therefore considered the proposed development would be acceptable and comply with Policies EN1/2, RT4/6 and the principles of the NPPF.

### **Environmental sustainability**

The ELR have a commitment to incorporating sustainable design features and are keen to explore initiatives for reducing the carbon footprint. It is therefore proposed to incorporate sustainable drainage measures, low carbon energy, install solar panels to the roof of the new builds and utilise surface water run-off from both the new buildings and the Listed Building.

This approach is welcomed.

**Impact on the surroundings** - The nearest properties are the residential dwellings on Baron Street, the closest being the flatted development directly adjacent to the main site entrance.

The ELR first use and development of Buckley Wells dates from the opening of the line in 1987 and since then has been used to house and maintain the early stock of carriages in the open air, which has developed to become one of the most acclaimed centres within the railway sector. Buildings and storage facilities have been added over time and large loader vehicles carrying the locomotives have been delivered to and from the site for a number of years, so local residents already experience a degree of noise and activity on the site plus the comings and goings of traffic.

Whilst the proposed development would involve more built development by the erection of a new separate carriage shed and extension to the Listed Building, these would provide better working conditions, storage/garaging accommodation and facilities for works to, and storage of the existing engines, carriages and other such associated machinery, and would not necessarily involve further intensification of the use of the site than currently exists. In terms of deliveries and traffic, again, this already occurs and the developments would likely not exacerbate the current arrangements. The works proposed to Baron Street would effectively improve access along the road and past the site, by increasing the width of the road and providing a pedestrian footway.

In terms of the appearance of the site, the removal of the palisade fence along Baron Street, the 'garaging' of locomotives and the general rationalisation of the site to relocate the storage containers and provide a formal parking area would tidy up and improve the appearance of the site overall.

The extension to the Listed Building would be more than 40m away from the flats and mostly screened by the existing LB. The new carriage shed would be over 200m away to the south west and significantly distanced so as not to harm outlook.

The main disruptions would more likely be from the construction of the development, where impacts can be minimised by conditions and legislation under the Environmental Protection Act.

It is therefore considered that the proposed development would not have a significantly harmful impact on the amenity of the nearby occupiers, and moreover so would seek to better the operations and appearance of the site.

As such, it is considered the proposed development would comply with policies EN1/2 and RT4/6 and the principles of the NPPF.

## **Highways**

The part of Baron Street which serves the site is unadopted and classified as a By Way Open to All Traffic. The road surface along this stretch is a combination of tarmac and cobbles. There is currently no separate pedestrian footway along this route.

Proposals include widening the part of Baron Street which serves the site to provide a 5.5m wide carriageway and 1.8m wide footway on the southern side of the road. To facilitate these works, the stone wall would be repositioned slightly further into the ELR land to the north. The existing palisade fence which also forms part of the boundary to the ELR land would be removed and replaced with a more appropriate and less visually intrusive paladin type for additional security.

Generally speaking, the access arrangements for the low loaders and other HGV vehicles would remain broadly similar to the existing arrangements.

Deliveries to the site of locomotives by low-loader vehicles currently take place at the westerly access point which is formed by a set of gates and leads to a large piece of land



next to the Listed Building. As it is proposed to site the extension to the LB closer to this access, an area on the northern side of Baron Street opposite the entrance would be used, with boundary fences and gates re-aligned to facilitate the manoeuvres of a low-loader vehicle. This pushes deliveries away from residential properties.

Deliveries of the larger locomotives occur on a relatively infrequent basis (currently 10-12 a year and projected to be 10-15 a year following the development) and it is therefore considered these arrangements would be acceptable. The Highway Section have raised no concerns or objections in this respect.

The access which is located to the east of the site is used by staff, volunteers and visitors and the smaller delivery of goods. This would be retained and serve a car park which would be formally laid out to provide just under 80 spaces including disabled spaces and EV charge points. This would be more than adequate on a day to day basis where there would not usually be more than 35 staff on site. Special events would require more staff on site but there would be more than adequate parking facilities especially factoring in the northern part of the Buckley wells site which has the capacity provide circa 30 spaces.

It is therefore considered that the proposed works to widen Baron Street and provide a public footway would considerably improve movement along the road for pedestrians whilst enabling the safe delivery of the large locomotives by low-loader vehicles to the site. Improvements to the parking arrangements would also benefit the site and its users.

The Highway Section have raised no objection in principle subject to details which can reasonably be addressed by conditions.

As such, the proposed development would comply with Policies EN1/2, HT2/4 and HT6/2 and the principles of the NPPF.

### **TfGM response**

#### Trip Generation

The Transport Statement states that the extra number of personnel will not have an impact but TfGM note that it is not clear if the extra visits will be considerable or not. TfGM would suggest that further evidence is provided in terms of peak hour travel.

The applicant has responded as follows -

Of the proposed staff and visitors on site day to day (20 + 14 respectively), from the staff travel surveys undertaken in January 2020 it appears that 89% travel by car and there is some flexibility in times of arrival and departure. As a worst case scenario, even if all of these trips were within the peak hours of 0800-0900 and 1700-1800, this equates to 30 vehicle trips. Some of these staff and volunteers are already using the site and making these journeys.

On special event days there may be a further 8 volunteers on site. This level of traffic generation does not warrant any further investigation and does not represent a severe impact on the road network. The introduction of a Travel Plan will introduce measures to reduce the use of the single occupancy car and encourage car sharing, cycling, walking and use of public transport to reach the site.

The additional car parking will be utilised on occasion by staff and volunteers from across the whole ELR, to free up parking at stations when large events are on.

The timing of the servicing of the site, particularly for the infrequent abnormal loads, can be conditioned if necessary.

TfGM note this response and advise that consideration be given to large events which may have an impact on the highway network locally if occurring during weekday peak hours. This would be managed by ELR.

#### Site Sustainability

On the issue of the accessibility of the site, the Transport Statement includes a detailed

breakdown of the accessibility of the site by foot, cycle and public transport. As the site is located on the edge of Bury Town Centre it is in a sustainable location with excellent accessibility and transport connections.

### **TfGM - Metrolink**

The area edged red includes a large area of land that is owned by TfGM and whilst this does not preclude the works from proceeding TfGM advise the applicant to expedite the conclusion of the legal agreement currently being progressed.

Whilst it is considered that the development proposals are unlikely to affect Metrolink operations, the site does adjoin the Metrolink boundary and therefore advise the applicant to consult the safety at work information which is available.

Metrolink also request that the existing access arrangements for the maintenance of Metrolink infrastructure should remain available at all times during construction unless specific prior agreement has been reached with Metrolink. Advisory notes to the applicant are recommended.

### **Ecology**

Following the submission of the habitat and biodiversity report, on the request of GMEU, a bat scoping report and additional biodiversity report have been submitted.

GMEU's original comments for the submission of this information have now been updated and their response is as follows.

#### Summary

GMEU are satisfied that ecological matters can be resolved by conditions or informatives.

#### Bats

The carriage shed was assessed as having low bat roosting potential by a suitable ecological consultant. This is because whilst there are numerous crevices for bats, these do not lead to any voids, the building is well lit by roof lights and unlined, with no roof void and the building subject to heavy engineering works. In line with best practice one emergence survey has been recommended.

GMEU accept that the building is very unlikely to be an important roost site, but the numerous minor roost features make it high risk for day roosts. Day roosts on large building such as this can however be very difficult to locate with reasonable avoidance measures often a better way forward.

In this instance however the consultants have also noted that whilst refurbishment of the listed building forms part of the planning application, no funding is actually available, the funds that are in place for the phase 1 and phase 2 parts of the development, the new build, which do not impact on this building. Funds will only be applied for, when and if planning permission is in place and may not in any event be successful. There is no likelihood of works commencing in 2022.

GMEU therefore accept that any dusk emergence survey at this time would not be valid by the time development came forward and likely in any event to be inconclusive given the scale of the building. It would therefore be unreasonable to request a survey that would be likely both inconclusive and invalid by such time as development commenced.

GMEU are therefore willing to depart from best practice and recommend a condition be applied to any permission for the submission of bat emergence surveys prior to the refurbishment of the building.

#### Nesting Birds

The consultants were informed that feral pigeons nest in the building. Pigeons are capable of breeding throughout the year. Nesting pigeons have legal protection under the terms of

the Wildlife & Countryside Act 1981 (as amended) but are also listed as a pest species and a 'general license' can be applied for from Natural England to allow work to proceed that would damage pigeon's nests, if present. GMEU therefore recommend a condition be applied to any permission for a detailed bird nesting survey to be carried out prior to refurbishment works.

#### Badgers

A survey has now been provided that found no evidence of badgers. This is in-line with expectations. GMEU are however aware of badgers now been present in the wider areas, it is therefore possible given the long timescale associated with the various phases of the development that badgers could colonise but are however of the opinion that this risk is low. It is therefore recommended that the survey is updated if earthworks are delayed more than 12 months from this survey carried out on the 4th October 2021. This would be a condition.

#### Invasive Species

A survey of the site has confirmed the presence of himalayan balsam and japanese knotweed in close proximity to the site. No giant hogweed was located. Measure to control the japanese knotweed are recommended but none proposed for the balsam. Whilst accepting that eradication is unlikely to be feasible for balsam, given the seed bank and presence of balsam on adjacent land, this still does not absolve the developer from taking reasonable measures to prevent spreading this species off-site ie carrying out localised control and biosecurity measures during construction.

A condition is now recommended for the submission of a method statement detailing eradication and/or control and/or avoidance measures for himalayan balsam and japanese knotweed.

#### Contributing to and Enhancing the Natural Environment and the existing Wildlife Corridor

Section 174 of the NPPF states that the planning policies and decisions should contribute to and enhance the natural and local environment.

GMEU accept that the reports general recommendations that mitigation through measures along the Bury Grammar School boundary are likely to provide net gain.

The footprint of the development site has generally negligible ecological value but does form part of a wildlife corridor that follows the route of the ELR and Metrolink through the Borough and to Heywood, this section through Bury being a weak point in the corridor. The development therefore provides an opportunity to strengthen the Wildlife Corridor through native planting along the boundaries of the development site and enhancement of the existing habitat to either side of the development platform.

Mitigation for loss of bird nesting habitat and potentially bat roosting habitat should also be provided. Given the generally low ecological value of the site and large area of land available to the ELR for mitigation to occur, GMEU are satisfied that the detail can be resolved via a condition along the following lines.

Prior to development an ecological mitigation and enhancement plan shall be submitted which shall include but not be restricted to:

- Mitigation for loss of trees;
- Mitigation for loss of bird nesting and bat roosting habitat and;
- Demonstrating enhancement of the Wildlife corridor

Subject to conditions, GMEU are satisfied that ecological issues can be resolved and are acceptable.

#### **Coal Authority**

Confirm that the application site falls within the defined Development High Risk Area; therefore within the application site and surrounding area there are coal mining features and hazards which need to be considered in relation to the determination of this planning application.

The Coal Authority records indicate that the site is likely to have been subject to historic unrecorded underground coal mining at shallow depth and that thick coal seams outcropped across the site. Records also indicate the presence of one recorded mine entry (shaft) within the planning boundary.

In support of the planning application, the applicant has submitted both an Interim Ground Investigation & Risk Assessment Report (25 October 2006, prepared by Leyden Kirby Associates Ltd) and a letter on a proposed foundation solution in lieu of further site investigation and remedial works. It is noted that the Interim Ground Investigation & Risk Assessment Report confirms that because of extensive investigation the recorded mine entry in the approximate area of the Running Shed and Heavy Maintenance poses no undue risk to the development.

Turning to the situation about potential shallow coalmine workings implicating on ground stability, the Coal Authority accepts that because of the existing operating use, further extensive intrusive investigation would not be practical. Consequently, and having considered the structural precautions proposed in lieu of further investigation, the designs of which have been informed from the very results of the site investigation, the Coal Authority is satisfied the risks posed to the development following their implementation would be very low. This is further reinforced when considering their relative lightweight construction.

The Coal Authority has no objections to this planning application and it will be the responsibility of both the developer and the Building Regulations process to ensure that the foundations proposed are installed accordingly.

**Drainage** - United Utilities have been consulted on the application and raise no objections to the proposed development and recommend a condition for the submission of a drainage scheme.

## **Objection**

### Rossendale Council objection - Summary

Rossendale Borough Council has submitted an objection to the proposal. Rossendale BC recognise the value of the railway in terms of the role it plays in the cultural and visitor economies of Rossendale, Bury and Rochdale and that the proposal would, in general terms, bring positive additions to the railway, widen its appeal and enhance the engine and carriage shed facilities to the benefit of the railway as a whole.

However, Rossendale BC is concerned that the application has been submitted without prior due diligence and dialogue with them. They are concerned that the proposal will undermine aspirations for a rail link from Rawtenstall to Manchester along the route of the railway and, in particular, one of their leading options for a tram/train interchange at the junction of the East Lancashire Railway and Metrolink which would require a single, uninterrupted track between Bury Bolton Street station and Buckley Wells Interchange.

To avoid this, Rossendale Council suggest that the proposed new build carriage shed and shunting lines would need to move approximately 5 metres in an easterly direction to allow space for the dedicated connection line and also request that these applications be referred to the Secretary of State for his decision due to their implications on future strategic transport infrastructure in the region.

## **Responses to objection**

### Response by Bury LPA

An objection has been submitted by Rossendale Borough Council regarding the perceived impact of the application on proposals for a commuter rail line connecting Rossendale with Manchester, which includes a route through the Buckley Well site. The objection claims that this commuter line would be undermined by the proposal.

However, there are no specific plans for the commuter line and there is no provision within

the development plan that can be taken into account in considering this application.

Likewise, the proposal for a commuter line does not feature within the Greater Manchester 2040 Transport Plan, which was recently endorsed by the Council and the Combined Authority.

Whilst it is acknowledged that Rossendale Council has historical and current aspirations to provide a commuter line through Bury, at this time there is no evidence that such a proposal is feasible, viable or deliverable. Therefore, very little weight can be attached to this objection.

In terms of Rossendale's request for the application to be referred, the Council is required to consult the SoS when it intends to approve certain types of application. The Buckley Wells application does not fall within any of the criteria in the Direction that would require consultation with the SoS.

The Direction does not affect the SoS's general power under S.77 to call in any planning application and anyone can ask the SoS to call-in a planning decision. The policy of the SoS, in general, is to only consider using call-in powers if planning issues of more than local importance are involved.

If Rossendale Council wish the decision to be determined by the SoS instead of Bury Council, they would need to ask the SoS and set out the reasons why they think it meets the criteria. It is not for Bury Council to ask the SoS to make the determination.

#### Response to objection from ELR - Summary

The East Lancashire Railway have submitted a response to the objection from Rossendale Borough Council specifying that, in their view the ELR have carried out proper due diligence and fully engaged with Rossendale BC on the scope and development of the proposals for the Buckley Wells heritage rail depot. This engagement has been undertaken through the ELR Trust Board (which includes three representatives from Rossendale BC) who have given their unanimous support to the proposal and agreed a grant to cover the costs of professional fees needed to develop the planning application.

ELR also state that proposals for a commuter service have previously been considered to be unfeasible and do not form part of the plans of the relevant strategic transport bodies. The ELR have also highlighted that very considerable operational, technical, safety and legal issues would have to be addressed to deliver Rossendale BC's aspirations for a dedicated commuter line through the Buckley Wells depot.

ELR therefore believe that Rossendale BC's objection and aspirations for Buckley Wells have no relevance to the planning and listed building consent applications for the Buckley Wells heritage rail depot development.

#### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

**Recommendation:** Approve with Conditions

**Conditions/ Reasons**

1. The development must be begun not later than three years beginning with the date of this permission.  
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings -

**Plans:**

**C-0340**

01 Rev B (Existing Site Layout),  
02 (Existing Running Shed Plans),  
03 (Existing Running Shed SW, NE Elevations & Sections)  
03-1 (Existing Running Shed SE & NW Elevations)  
08 Rev H (Proposed Site Layout)  
09 Rev C (Proposed Baron Street Improvement Works)  
10 Rev B (Proposed Phasing Plan)  
12 (Existing Ground Floor & Roof Plan)  
100 Rev E (Proposed Carriage Shed Plan)  
101 Rev D (Proposed Carriage Shed SW, NE Elevations & Section A-A)  
101-1 Rev A (Proposed Carriage Shed SE & NW Elevations)  
200 Rev D (Proposed Running Shed Ground Floor Plan)  
200-1 Rev A (Proposed Running Shed First Floor Plan)  
201 Rev B (Proposed Running Shed SW, NE Elevations & Section B-B)  
201-1 Rev B (Proposed Running Shed SE & NW Elevations)  
202 (Proposed Running Shed Elevation Details)  
SLP Rev B (Site Location Plan)

**TRI-2480-03-A1** (Existing Internal Elevations)

04 (Existing Sections)

**TRI-2313 TRI-2313-01-A1** Existing Site Layout

02 A1 Existing Site Layout  
03 A1 Existing Site Layout  
04 A1 Existing Site Layout  
05 A1 Existing Site Layout  
06 A1 Existing Site Layout  
07 A1 Existing Site Layout  
08 A1 Existing Site Layout  
09 A1 Existing Site Layout  
10 A1 Existing Site Layout  
11 A1 Existing Site Layout

**Supporting Documents:**

Additional Biodiversity Report by Blackfryers 20 Dec 2021 Final  
Bat Scoping Report by UES 1 Dec 2021 ref UES 503532/01  
Summary of proposed works Rev A  
190626 ELR habitat report dated 25 June 2019 by Blackfryers  
Structural Report  
Interim Ground Investigation & Risk Assessment Report  
PAS00020 ADVICE- Coal Authority  
NX397 Coal Mining Risk Assessment Final  
SCP\_18638\_Transport Statement\_16.12.20\_v3issue-by SCP  
Appendix 6.3 Timberwise Survey Report  
Survey of the Fabric- Buckley Wells- 1-302  
Combined Heritage and Design & Access Statement

and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. No development shall commence unless and until:-
- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
  - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
  - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

5. Prior to occupation the EV chargepoints shown on approved plan C-0340-08 rev H shall be provided and shall thereafter be retained.

Reason - In accordance with the NPPF, to encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life.

6. No development shall commence until a surface water drainage scheme has been submitted to and approved in writing by the Local Planning Authority. The drainage scheme must include:

- (i) An investigation of the hierarchy of drainage options in the National Planning Practice Guidance (or any subsequent amendment thereof). This investigation shall include evidence of an assessment of ground conditions and the potential for infiltration of surface water;
- (ii) A restricted rate of discharge of surface water agreed with the local planning authority (if it is agreed that infiltration is discounted by the investigations); and
- (iii) A timetable for its implementation.

Details of proposed maintenance arrangements should also be provided. The approved scheme shall also be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards.

The development hereby permitted shall be carried out only in accordance with the approved drainage scheme.

Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

7. Foul and surface water shall be drained on separate systems.  
Reason. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
8. In the peak activity season prior to refurbishment works commencing bat emergence surveys shall be carried out and supplied to the Local Planning Authority and a reasonable avoidance measures method statement for bats produced and agreed in writing by the Local Planning Authority.  
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.
9. No refurbishment works shall occur unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to works and written confirmation provided that no active bird nests are present which has been agreed in writing by the LPA unless the species present is feral pigeon, in which case a general license issued by Natural England authorising destruction of feral nests should be provided to the LPA.  
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
10. If the development hereby approved does not commence before 1st October 2022, a re-survey of the site and within 30m of the earthworks for badger setts will occur and the findings supplied to and agreed in writing by the LPA.  
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
11. Prior to any earthworks a method statement detailing eradication and/or control and/or avoidance measures for Himalayan Balsam and Japanese Knotweed should be supplied to and agreed in writing to the LPA. The agreed method statement shall be adhered to and implemented in full. The approved management plan shall include a timetable for implementation. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority.  
Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
12. Prior to development an ecological mitigation and enhancement plan including but not restricted to:
  - Mitigation for loss of trees;
  - Mitigation for loss of bird nesting and bat roosting habitat and;
  - Demonstrating enhancement of the Wildlife corridorThe approved scheme shall thereafter be implemented not later than 12 months from the date the building(s) is first occupied or within the first available tree



planting season,; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies EN6/4 - Wildlife Links and Corridor, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

13. Prior to the removal of the boundary wall and/or palisade fence to Baron Street, details of the proposed boundary treatment to Baron Street shall be submitted to and approved by the Local Planning Authority. The approved details only shall be implemented and thereafter retained.

Reason. In the interests of visual amenity and highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

14. The existing access arrangements to Metrolink infrastructure for maintenance purposes should be remain available at all times during construction and post construction unless specific prior agreement has been obtained from Metrolink.

Reason. To ensure that the development does not adversely impact Metrolink operations pursuant to Bury Unitary Development Plan Policies RT4/6 - East Lancashire Railway, EN1/2 - Townscape and Built Design and HT4 - New Development.

15. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.

Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

**Ward:** Bury East - Redvales

**Item** 02

**Applicant:** East Lancashire Light Railway Co. Ltd

**Location:** Buckley Wells Locomotive Depot, Baron Street, Bury, BL9 0TY

**Proposal:** Listed Building Consent for the development of the ELR's historic locomotive and carriage works site including repair and restoration of the 1857 locomotive work building, the construction of new locomotive running shed, incorporation of associated new workshops and training facilities and construction of a standalone carriage shed.

**Application Ref:** 66746/Listed Building  
Consent

**Target Date:** 17/05/2021

**Recommendation:** Approve with Conditions

**The Development Manager has recommended a site visit prior to the Planning Control Committee meeting.**

### **Description**

The application relates to Buckley wells which is the location and facility of the locomotive and carriageworks and rail depot for the East Lancashire Railway (ELR).

The site is located on Baron Street which is formed off Manchester Old Road. The eastern part of Baron Street is adopted and comprises residential properties either side and an apartment block located next to the site where Baron Street becomes an unmade and part cobbled unadopted road, classified as a by-way open to all traffic (106 BUR) at which point the main entrance on the site is formed to the south. Baron Street then continues westwards to St Gabriel's School.

The overall Buckley Wells site area is 9.1ha. The main operational part of the site is 5.5ha which is immediately to the south of Baron Street and comprises a Grade III Listed locomotive works/carriage shed building, an extensive network of railway sidings, storage containers, open air storage for a wide range of railway infrastructure and engineering components and parking areas.

This part of the site has a narrow elongated triangular form, with Baron Street forming the the northern boundary, the eastern boundary defined by the Metrolink line which is screened from the site by dense planting and to the west is a steep embankment. It is within a Wildlife Corridor and partly within a River Valley.  
This part of the site is the subject of this planning application.

The other areas of the Buckley wells site are located opposite, to the north of Baron Street which is additional operational land comprises a 'modern' carriage shed building constructed in the 1980's and further railway sidings. To the west is an area of un-used land set at a lower level to the main site which is overgrown with self-seeded vegetation.

The ELR plays a key role in as a visitor and heritage attraction in the north west and has its main workshop facilities located at the Buckley Wells site where the locomotives, carriages and wagons are housed and where the essential locomotive and carriage operations, maintenance and restoration facilities necessary to operate the successful heritage railway are carried out.

In recent years the ELR Company has been pursuing a scheme of works to help improve

visitor experience and Buckley Wells, as the centre for the railway operations, is the critical and indispensable base for the storage and maintenance of historic locomotives and carriages such as the Flying Scotsman.

The works are currently carried out in two buildings -

- a Grade II Listed carriage shed building dating from 1857 which is located immediately to the south of Baron Street. This houses the engineering facilities for the maintenance and restoration of historic locomotives and facilities for both long term restoration projects and operational locomotives in daily use. This building is deteriorating badly and in need of significant investment to secure its longevity not only as a valuable heritage asset but as vital facility for ELR and the site.
- a more modern carriage shed to the north of Baron Street which provides limited facilities for restoration works and storage and is challenged by site constraints.

Not only is there a need to carry out critical works to the Listed Building but there is also a need to provide additional purpose built provision for the fleet of locomotives and carriages so they can be maintained to the highest possible standards expected of such an established and high quality attraction.

There is also a need to expand the important and essential training facilities within the site to attract and train up future generations of volunteer engineers necessary and crucial to maintain and successfully operate the heritage railway for years to come.

In essence, development works are essential to facilitate the managed growth of the ELR on a long term and sustainable basis as a premier visitor and heritage attraction.

There are 5 key elements to the redevelopment of the Buckley Wells site -

1. A comprehensive programme of restoration and repair to the Grade II Listed historic Buckley Wells carriage shed building. It will continue to be used for the restoration of locomotives/general railway engineering operations including workshops and stores. An extension to provide new offices would be located at the north end of the building to support training, skills and various operations associated with the overall site.
2. An extension to the LB carriage shed to provide a new locomotive running shed and maintenance workshop which would be located on the western side of the building and on the footprint of a former original Buckley Wells engine shed (demolished in the 1970's). This new build would have a portal frame structure with steel clad elevations which would also accommodate classroom/workshop areas, stores, and other ancillary facilities.
3. The erection of a new carriage shed which would house, restore and repair the ELR's fleet of historic railway coaches. This would be located on an area of sidings to the south of the Listed Building, approx 90m away. The building would be vital to the storing of the machines and engines and for the protection from the elements and vandalism, particularly as their maintenance can take an extensive period of time. The building would be formed by a base of red engineering brick with steel clad elevations interspersed with a row of symmetrically aligned window openings. Solar panels would be located on the roof. A new internal access road would be formed to this building.
4. Within the site some tracks would be re-laid to facilitate rail access to the new buildings.
5. It is also proposed to rationalise the external part of the site to provide for the expanded operational capacity of the site and would include new areas of hardstanding, designated parking areas for staff and volunteers and external storage facilities in the eastern part of the site.

This application seeks Listed Building Consent for these works.

The scheme is proposed as a phased development comprising 3 components -

Phase 1 - Lifting and relaying rail track to establish facilitate and new track layout and rationalise the areas for storage around the site. The site would also be cleared for the construction works for the new build.

Phase 2 - Construction of the new build carriage shed and vehicular access route to the

building.

Phase 3 - Works to restore the Listed carriage shed and construction of the proposed running shed.

### **Relevant Planning History**

47480 - Workshop extension to carriage and wagon shed - Approve with Conditions 28/02/2007

49349 - Carrying out of an engineered fill operation prior to accommodating new rail track infrastructure without the restriction within condition 10 of planning permission ref. 48268 that would limit the hours during which the importation of materials to carry out the operation can take place. - Approve with Conditions 19/03/2008

49958 - Erection of 2.4 metre high paladin security fencing from the Metrolink boundary along the length of the bury grammar schools playing fields access road boundary, and returning along baron street to the east Lancashire railway level crossing (retrospective) - Approve 19/09/2008

53647 - Renewal of planning permission 49349 to carry out engineering fill operation prior to accommodating new rail track infrastructure - Approve with Conditions 20/04/2011

### **Publicity**

Letters sent to 122 properties on 30/3/21.

Site notice posted 15/4/21

Press advert 1/4/21

No representations made by the public have been received.

### **Statutory/Non-Statutory Consultations**

**Conservation Officer** - No objection

**G M Archaeological Advisory Service** - No objection

**Historic England** - No objection

**Rossendale Borough Council** - **Objection received.** This is referenced in parallel application reference 66745

**Pre-start Conditions** - Applicant/Agent/has agreed with pre-start conditions

### **Unitary Development Plan and Policies**

NPPF	National Planning Policy Framework
EC1/2	Land Suitable for Business (B1)
EN1/2	Townscape and Built Design
EN2/3	Listed Buildings
EN6/4	Wildlife Links and Corridors
EN7	Pollution Control
EN7/1	Atmospheric Pollution
OL5/2	Development in River Valleys
HT2/4	Car Parking and New Development
HT3/4	Schemes to Assist Metrolink
HT4	New Development
HT6/2	Pedestrian/Vehicular Conflict
RT4/6	East Lancashire Railway
SPD11	Parking Standards in Bury
Area	Bridge Road/Buckley Wells
BY2	

### **Issues and Analysis**

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning

considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

## **Policies**

Planning (Listed Buildings and Conservation Areas Act) 1990 Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 require decision makers to have special regard to the desirability of preserving a listed building or its setting or any feature of architectural or historic interest it possesses. The decision maker must also give considerable importance and weight to the desirability of preserving the setting of the listed building.

The following policy of the adopted Bury Unitary Development Plan is relevant:  
EN2/3 - Listed Buildings

The Council will actively safeguard the character and setting of Listed Buildings by not permitting works, alterations or changes of use which would have a detrimental effect on their historical or architectural character and features. Proposals for demolition will be opposed and will only be considered where it is demonstrated conclusively that the building(s) cannot be retained.

In considering applications for Listed Building Consent, the Council will have regard to the following criteria: a) the impact of the proposal on the historic fabric of the building;

- the relationship of any extension to the Listed Building in terms of its height, size, design, and roofscape;
- the need to protect the setting of the Listed Building;
- the impact of associated ancillary facilities and infrastructure works.

The setting of a Listed Building is often an essential feature of its character; unsympathetic development in close proximity to a Listed Building can mar its appearance, make its future use unattractive or untenable, or physically damage its structure. It is therefore important to consider the effect that a proposed development may have on such buildings.

## **National Planning Policy Framework (NPPF) –**

The following paragraphs from Chapter 16 of the NPPF – Conserving and Enhancing the Historic Environment and relevant:

194. In determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.

195. Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

197. In determining applications, local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable

communities including their economic vitality; and

c) the desirability of new development making a positive contribution to local character and distinctiveness.

199. When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

200. Any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification. Substantial harm to or loss of:

- a) grade II listed buildings, or grade II registered parks or gardens, should be exceptional;
- b) assets of the highest significance, notably scheduled monuments, protected wreck sites, registered battlefields, grade I and II\* listed buildings, grade I and II\* registered parks and gardens, and World Heritage Sites, should be wholly exceptional.

201. Where a proposed development will lead to substantial harm to (or total loss of significance of) a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or total loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- a) the nature of the heritage asset prevents all reasonable uses of the site; and
- b) no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation; and
- c) conservation by grant-funding or some form of not for profit, charitable or public ownership is demonstrably not possible; and d) the harm or loss is outweighed by the benefit of bringing the site back into use.

202. Where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

The NPPF at Annex 2 describes setting as:

Setting of a heritage asset: The surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance or may be neutral;

and

Significance (for heritage policy): The value of a heritage asset to this and future generations because of its heritage interest. The interest may be archaeological, architectural, artistic or historic. Significance derives not only from a heritage asset's physical presence, but also from its setting. For World Heritage Sites, the cultural value described within each site's Statement of Outstanding Universal Value forms part of its significance.

### **Heritage Values and Significance –**

The applicant has provided a comprehensive combined heritage and design and access statement. This document sets out the significance of the shed, the requirement for increased and improved facilities and the impact of the proposal on the buildings' significance.

The following has been reproduced in part from the heritage values and significance part of

the document.

**Evidential Value** – English Heritage (2008) suggests that "Evidential value derives from the potential of a place to yield evidence about past human activity": Buckley Wells has operated continuously as a locomotive depot since 1846. The site has adapted continuously over the 173 year history of its use keeping it relevant as an operational locomotive depot. Evidence of past development and evolution of the site as an operation loco depot can be derived from four primary sources: archive material, historic mapping, archaeology and photographic records.

**Historical Value** - English Heritage (2008) suggests that - "Historical value derives from the ways in which past people, events and aspects of life can be connected through a place to the present. It tends to be illustrative or associative": Buckley Well's principal historical value lies in the fact that it is an extremely rare example of a still operational historic locomotive works and depot with the use of the site dating back to 1846 placing it in the early phase of railway development nationally and internationally.

The surviving building, the Listed 1857 loco works/carriage shed building, is the first purpose built permanent building to be constructed on the site. This places it relatively early within the development of the railways and therefore now as (potentially) the oldest remaining operational locomotive works building in Europe.

Throughout its long history the site and the building have been associated with important technological advances in locomotive and rail infrastructure development.

**Communal Value** - English Heritage (2008) suggests that: "Communal value derives from the meanings of a place for the people who relate to it, or for whom it figures in their collective experience or memory": There are several aspects to the ways in which communal value can be attributed to the Listed 1855/57 loco works/carriage shed building and the wider historic Buckley Wells locomotive depot site.

Early communal value would mainly be gained from the local work force that were employed in the building. The building was not part of the public realm but solely an engineering workplace. Over the last 35 years since the successful formation of the East Lancashire Steam Railway the building has taken on a distinct and new sense of communal value as follows:

- The building is now used by a new local workforce (circa 90 personnel) which use the building on a full and part-time basis as they restore both engines and carriages.
- The building is now visited by various groups and railway enthusiasts as they seek to learn and share engineering skills. Some of these volunteers have been associated with this building for over 30 years.
- For visitors to the ELR heritage railway the communal value is largely hidden but vital. Buckley Wells provides the essential engineering and operational support infrastructure that enables visitors to enjoy and experience historic steam and diesel locomotives and passenger carriages (which are heritage assets in their own right).
- For the active rail enthusiast (be it volunteer or professional engineer) the historic 1857 loco works/carriage shed building and wider Buckley Wells site affords major communal value. Specifically, the opportunity that site provides for people to learn and practice a wide range of critical heritage engineering skills and activities directly connects today's volunteers and staff working out of the site with the past generations of rail engineers working stretching back to the earliest days of the development of the railways.

**Aesthetic Value** - English Heritage (2008) suggests that: "Aesthetic value derives from the ways in which people draw sensory and intellectual stimulation from a place". The Buckley Wells site's aesthetic value derives principally in two areas:

- External aesthetics. The building presents itself as a functional Victorian Railway Shed built with simple load bearing construction. It is built on a regular pattern of simple bays which are expressed externally with brick pilasters, with a combination of glazed and blind openings. The detailing of the building is enhanced with brickwork corbelling and brick arches.

- Internal aesthetics. The building retains its principle skeletal structural of internal load-bearing columns with timber king-post and queen-bolt trusses. The internal architecture remains in its original form and presents a virtually unspoilt aesthetic of the original Victorian railway building.

The statement of significance concludes the site is the operational locomotive depot for the ELR heritage railway whose use as a locomotive depot dates back to 1846, and where the surviving building, the Grade II Listed Locomotive Works/Carriage Shed building, is both the first (and largest) purpose built building to be constructed on the site with the oldest parts of the building dating from 1855-1857. Dating from the early period of railway development it is believed to be the oldest surviving fully operational rail depot and locomotive workshop in Europe.

The site's greatest significance therefore lies in the survival of the 1857 building as an example of early industrial railway architecture and in the continuity of the use of the building and site as an operational rail depot and locomotive works since 1846. This survival is all the more remarkable because industrial railway architecture (and especially locomotive works) is a very neglected part of the nation's industrial architectural heritage, with most of the country's largest and most important historic locomotive works already lost to redevelopment.

Through the assessment of the site and the distillation of the listed buildings' heritage values it is considered the site and buildings possess overall moderate to high heritage significance. There are some elements that possess minimal or low significance such as external storage, modular buildings, non-historic rainwater goods, the extension to the southern end of the shed which was part demolished in 1974 and the office extension built in 1862 and is described in the Historic England listing description as not of special interest.

#### **Impact of the proposals on the Listed Building -**

The proposal comprises of the extension of the existing 1857 building on its west elevation which would replace the previous extension built in 1875 and demolished in the 1970s. This would comprise of a two bay 4 road running shed. It would be a steel portal construction with a red brick plinth and steel cladding system. The roof would be hipped with a ventilation louvre along its length with solar panels and a translucent rooflight.

In design terms the proposed extension draws inspiration from the original extension in terms of its size and scale with an acknowledgement to the architectural style of the original extension with its 2 bay construction and ventilation louvre. In terms of height the proposal would not dominate the existing shed and would not create unacceptable massing. Overall, the proposed extension does not attempt to be a pastiche of the original shed and as such it does not challenge the architectural aesthetic of the listed building or confuse its legibility as an historic building of merit and interest.

The proposed extensions to the 1862 offices at the north elevation of the shed also draws inspiration from the existing structure by virtue of its materials and height. Although part of the listing entry, this element does not possess any special interest. The proposed office extension would provide much needed welfare, office and training facilities.

The proposed stand-alone carriage restoration workshop, carriage/diner and catering accommodation shed would be sited to the south west of the 1857 shed and would be within its setting. Its design philosophy is the same as that in approach to the design of the 1857 shed extension. It would be a portal frame 2 bay construction with brick plinth and a steel cladding system, ventilation louvre, solar panels. Its height at approximately 16 metres would be approximately 4 metres higher than the highest part of the original 1857 shed. This would have an impact on its setting.

#### **Setting of the Listed Building -**

The setting of a listed building are the surroundings in which a heritage asset is experienced. Its extent is not fixed and may change as the asset and its surroundings evolve. Elements of a setting may make a positive or negative contribution to the



significance of an asset and may affect the ability to appreciate that significance or may be neutral.

It is important to note that setting is not a land use or a heritage value. Development within or change to a setting can impact on the significance of the listed building. Buckley Wells is an industrial site by its nature, its setting has changed over its lifetime with the addition and removal of extensions to the original shed as well as addition and removal of permanent and semi-permanent outbuildings. The site currently has a number of cabins and carriages used as office space and for welfare and other ad hoc storage facilities within the site and adjacent to the east elevation of the shed which have a negative impact on the setting of the listed building. It is understood that proposals would allow these elements to be removed. This would have a positive impact on its setting.

#### **Heritage Balance and Conclusion -**

Paragraph 201 of the NPPF states that where proposals amount to substantial harm to or total loss of significance to a heritage asset the application should be refused. It is considered the proposals would not amount to this level of harm. The proposed extension to the shed and office will not require any significant loss of historic fabric of the shed or alter its use or adapt it in any way that would cause loss of significance as an architecturally important railway building or the continuing use of the site as a railway depot. The proposed extension would replace one that previously existed historically and as such together with the office extension would represent a new addition to the building. This would represent less than substantial harm within the terms of paragraph 202 of the NPPF. As such paragraph 202 of the NPPF requires that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

It is considered the proposed extensions would bring considerable public benefits in protecting the long term viable use of the shed and site as heritage railway and which would also be compatible with its conservation. It would facilitate the expansion of the heritage railway in a viable and compatible way and using green energy securing its future as well as bringing economic and educational benefits. As such the proposed extensions would outweigh, by virtue of their public benefits, the less than substantial harm identified and therefore would comply with paragraph 202 of the NPPF.

The proposed stand-alone carriage restoration workshop, carriage/diner and catering accommodation shed would be sited to the south west of the 1857 listed shed and would be within its setting. The size and proximity of the proposed building together with associated works comprising of additional hardstanding and track layout would have an impact on the setting of the listed 1857 shed. The question is whether or not the proposal would alter the setting in such a way as to cause harm to the significance of the listed shed.

The site is an evolved and evolving industrial site. As a working railway depot, it is viewed in close by and wider distances within its industrial site context. The proposed stand-alone carriage shed would not alter this fact. However, the way in which the listed shed is experienced would be altered by the proximity of the proposal but given the context of the site this alteration would not lead to substantial harm to or total loss of significance with regard to paragraph 201 of the NPPF. Therefore, the proposed stand-alone carriage shed would represent less than substantial harm to the significance of the listed shed. This harm would be outweighed by the considerable public benefits brought by the scheme as already identified above.

The proposal would therefore preserve the identified special historic and architectural significance of the Grade II listed Buckley Wells locomotive and carriage shed and its setting.

#### **Internal and External Repairs to the Grade II Listed Shed -**

A comprehensive structural and condition report with a schedule of repair/restoration has

been submitted as part of the listed building consent application. Careful consideration will be given to the amount, scale and scope and level of intervention of the required repairs/restoration. As a rule, repairs will be kept to the required minimum in order to preserve the significance of the building and this will be balanced against the need to preserve the fabric of the building with regard to air and water tightness and structural integrity.

The repairs and restoration are acceptable and comprise of external roof repairs, window repairs, external brick repairs and internal roof timber repairs. Conditions would be imposed on any listed building consent for the formal assessment of condition and method of repair, scope and intervention to be submitted to and approved in writing by the local planning authority before the commencement of works for repair or restoration.

It is recommended that a condition is also imposed for the submission of details of the external finishes for the proposed extension and stand-alone carriage building to be submitted to and approved in writing by the local planning authority before any development commences.

### **Consultee responses**

**Historic England** - On the basis of the information available to date, HE do not wish to offer any comments and suggest that the views of the LPA's specialist conservation adviser are sought.

**Greater Manchester Archaeological Advisory Service** - The application is supported by a detailed Combined Heritage and D&A Statement, prepared by Equilibrium Architects Ltd, which sets out the historic development and interest in the site and presents a robust assessment of the impact on the significance of the listed building and its setting, as required by Section 16, Paragraphs 189 and 190, of the NPPF. The application is also supported by a detailed survey of the building's fabric and structure, which provides a comprehensive record of the buildings 'as existing'. The Bury Conservation Officer may wish to advice further on the listed building, but GMAAS is not seeking any further archaeological investigation of the historic built environment.

The application is not supported by an archaeological desk-based assessment that specifically considers the potential impact of the proposed development on below-ground archaeological remains. However, from a review of the historic mapping available for the site and the detail presented in the documentation supporting the application, it is evident that the impact on below-ground archaeology is unlikely to be anything other than negligible. As such, GMAAS does not consider it necessary to impose any archaeological requirements on the applicant.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

**Recommendation:** Approve with Conditions

### **Conditions/ Reasons**

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.  
Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.
2. This decision relates to drawings -

**Plans:**

**C-0340**

01 Rev B (Existing Site Layout),  
02 (Existing Running Shed Plans),  
03 (Existing Running Shed SW, NE Elevations & Sections)  
03-1 (Existing Running Shed SE & NW Elevations)  
08 Rev H (Proposed Site Layout)  
09 Rev C (Proposed Baron Street Improvement Works)  
10 Rev B (Proposed Phasing Plan)  
12 (Existing Ground Floor & Roof Plan)  
100 Rev E (Proposed Carriage Shed Plan)  
101 Rev D (Proposed Carriage Shed SW, NE Elevations & Section A-A)  
101-1 Rev A (Proposed Carriage Shed SE & NW Elevations)  
200 Rev D (Proposed Running Shed Ground Floor Plan)  
200-1 Rev A (Proposed Running Shed First Floor Plan)  
201 Rev B (Proposed Running Shed SW, NE Elevations & Section B-B)  
201-1 Rev B (Proposed Running Shed SE & NW Elevations)  
202 (Proposed Running Shed Elevation Details)  
SLP Rev B (Site Location Plan)

**TRI-2480-03-A1** (Existing Internal Elevations)

04 (Existing Sections)

**TRI-2313 TRI-2313-01-A1** Existing Site Layout

02 A1 Existing Site Layout  
03 A1 Existing Site Layout  
04 A1 Existing Site Layout  
05 A1 Existing Site Layout  
06 A1 Existing Site Layout  
07 A1 Existing Site Layout  
08 A1 Existing Site Layout  
09 A1 Existing Site Layout  
10 A1 Existing Site Layout  
11 A1 Existing Site Layout

**Supporting Documents:**

Additional Biodiversity Report by Blackfryers 20 Dec 2021 Final  
Bat Scoping Report by UES 1 Dec 2021 ref UES 503532/01  
Summary of proposed works Rev A  
190626 ELR habitat report dated 25 June 2019 by Blackfryers  
Structural Report  
Interim Ground Investigation & Risk Assessment Report  
PAS00020 ADVICE- Coal Authority  
NX397 Coal Mining Risk Assessment Final  
SCP\_18638\_Transport Statement\_16.12.20\_v3issue-by SCP  
Appendix 6.3 Timberwise Survey Report  
Survey of the Fabric- Buckley Wells- 1-302  
Combined Heritage and Design & Access Statement

and the development shall not be carried out except in accordance with the

drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. Details of the external finishes of the locomotive shed extension and stand-alone carriage shed hereby approved shall be submitted to and approved in writing by the Local Planning Authority before any development commences. Only the approved materials/bricks shall be used for the construction of the development.  
Reason. For the avoidance of doubt, in the interests of visual amenity and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.
4. Repairs/replacement to the roof covering shall be carried out in accordance with the submitted Survey of Fabric - C-0340 Revision A and relevant Grid Survey Reference, Timberwise Report - G62753-GF-CRW and Booth King Structural Engineer Report - 12889 using the existing natural slates and any replacements required shall be of the same type, size, thickness and colour of natural slate as the existing original natural slates.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.
5. Repairs/restoration/replacement of the internal roof timbers shall be carried out in accordance with the submitted Survey of Fabric - C-0340 Revision A and relevant Grid Survey Reference, Timberwise Report - G62753-GF-CRW and Booth King Structural Engineer Report - 12889.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.
6. Cleaning and re-pointing the internal and external brickwork shall be carried out in accordance with the submitted Survey of Fabric - C-0340 Revision A and relevant Grid Survey Reference. Raking out of brickwork shall be carried out by hand only and power tools shall not be used.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.
7. Samples of replacement brick to be used in any repairs shall be submitted to and approved in writing by the Local Planning Authority before the replacements are used.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.
8. Details of the repair, restoration, or replacement of timber window frames and

glazing shall be submitted to and approved in writing by the Local Planning Authority before works to repair the windows are started or windows replaced and shall be carried out in accordance with the submitted Survey of Fabric - C-0340 Revision A and relevant Grid Survey Reference and Timberwise Report - G62753-GF-CRW. Details must include a condition report of the existing windows, method statement of repair, details of design and section drawing showing window set back reveal where appropriate. The approved details only shall thereafter be implemented.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.

9. Details of replacement timber doors in accordance with Survey of Fabric - C-0340 Revision A and relevant Grid Survey Reference shall be submitted to and approved in writing by the Local Planning Authority before the replacements are used.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.

10. Details of the replacement western rooflight and lantern to the shed shall be submitted to and approved in writing by the Local Planning Authority before the rooflight or lantern is replaced and shall be carried out in accordance with the submitted Survey of Fabric - C-0340 Revision A and relevant Grid Survey Reference and Timberwise Report - G62753-GF-CRW. Details must include a plan showing the details of design. The approved details only shall thereafter be implemented.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.

11. Details of replacement gutters, downpipes and hoppers if appropriate shall be submitted to and approved in writing by the Local Planning Authority before the replacements are carried out. The approved details only shall thereafter be implemented.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of repair and to preserve the significance of the Grade II listed Buckley Wells Carriage Shed pursuant to adopted Bury Unitary Development Plan policy EN2/3 - Listed Buildings and paragraphs 194, 195, 197, 199, 200, 201 and 202 of the National Planning Policy Framework.

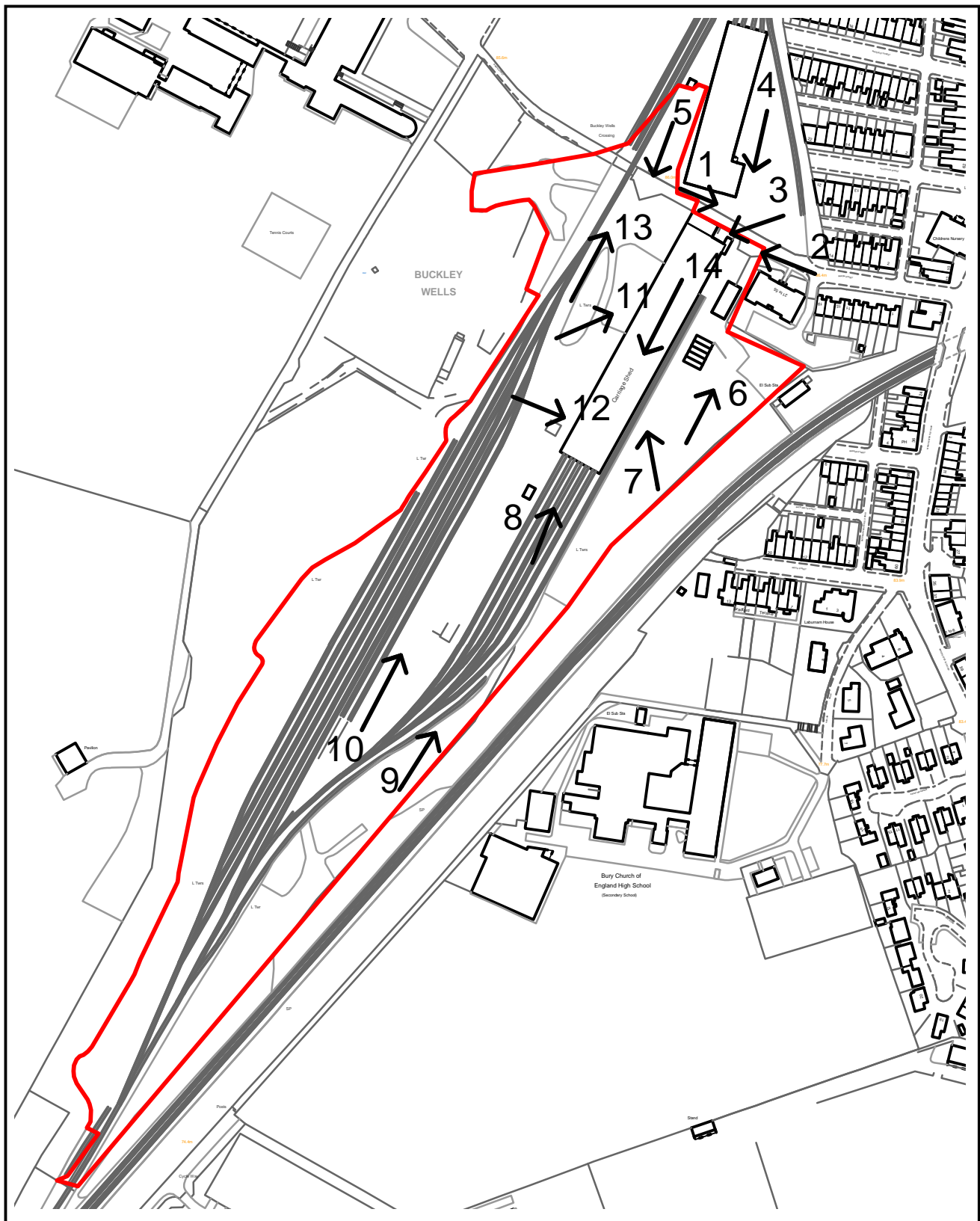
For further information on the application please contact **Jennie Townsend** on **0161 253-5320**







# Viewpoints



## PLANNING APPLICATION LOCATION PLAN

**APP. NO 66745**

**ADDRESS: Buckley Wells Locomotive Depot  
Baron Street**

**Planning, Environmental and Regulatory Services**

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**Bury**  
COUNCIL

66745

Photo 1



Photo 2





66745

Photo 3



Photo 4





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Photo 5



Photo 6





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Photo 7



Photo 8





66745

Photo 9



Photo 10





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Photo 11



Photo 12



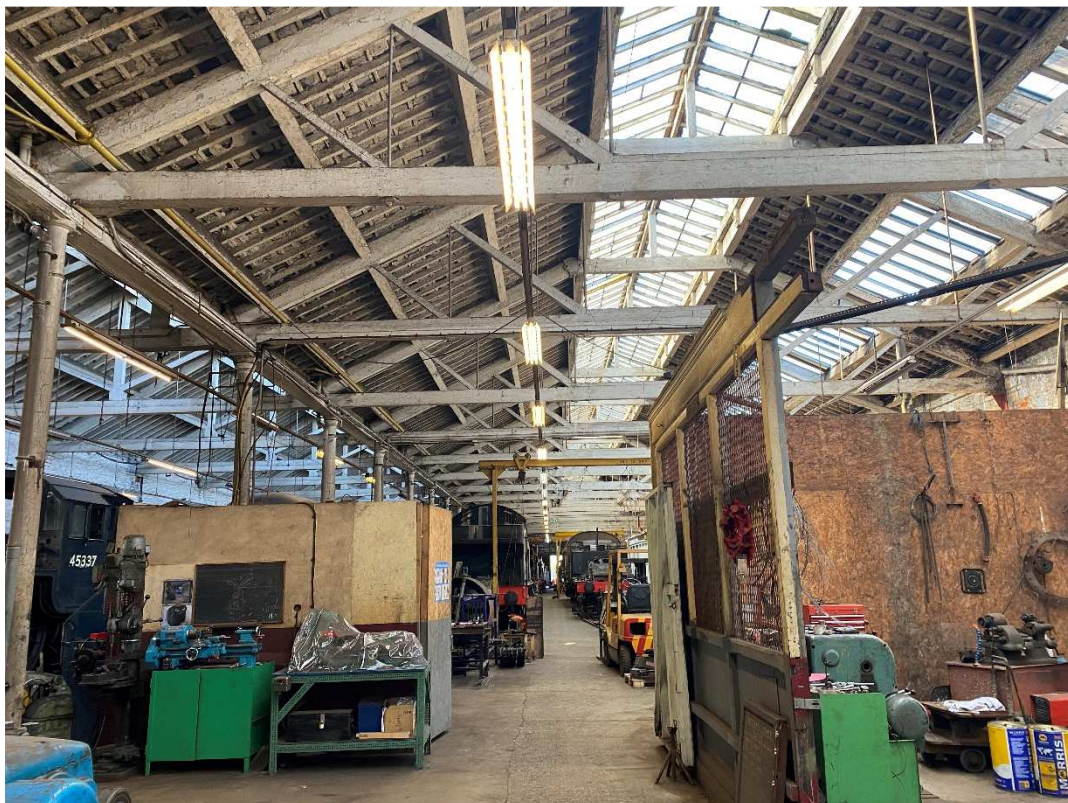


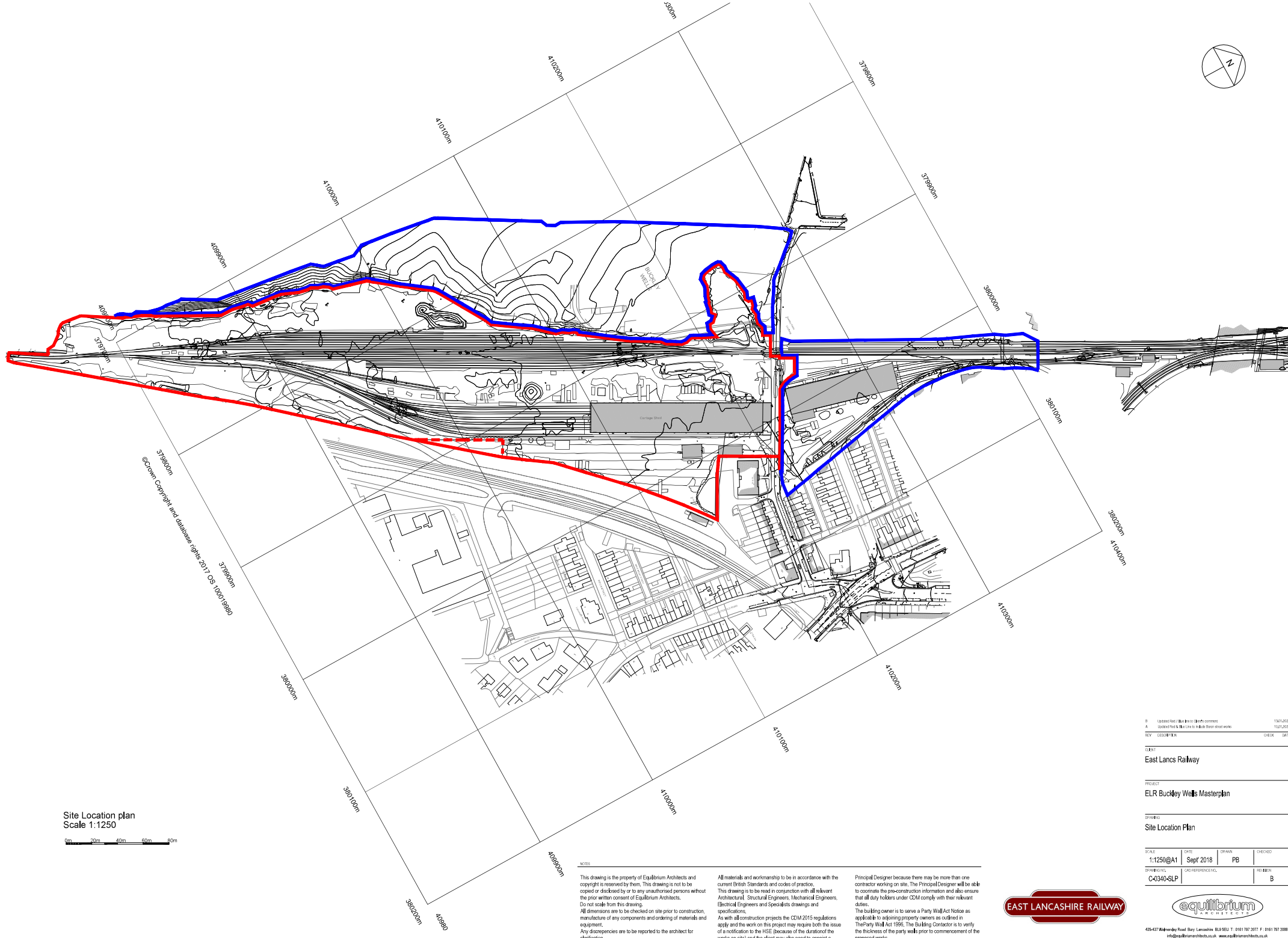
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Photo 13



Photo 14





B	Updated Plan to identify comments	13/01/2018
A	Updated Plan to identify comments	13/01/2018
REV	DESCRIPTION	CHECK DATE

CLIENT  
**East Lancs Railway**

PROJECT  
**ELR Buckley Wells Masterplan**

DRAWING  
**Site Location Plan**

SCALE	DATE	DRAWN	CHECKED
1:1250@A1	Sept 2018	PB	
DRAWING NO.	CAC REFERENCE NO.	REVISION	
C-0340-SLP		B	



435-437 Whitehead Road, Bury, Lancashire, BL9 9BU T: 0161 707 2077 F: 0161 707 2088  
info@equilibriumarchitects.co.uk www.equilibriumarchitects.co.uk

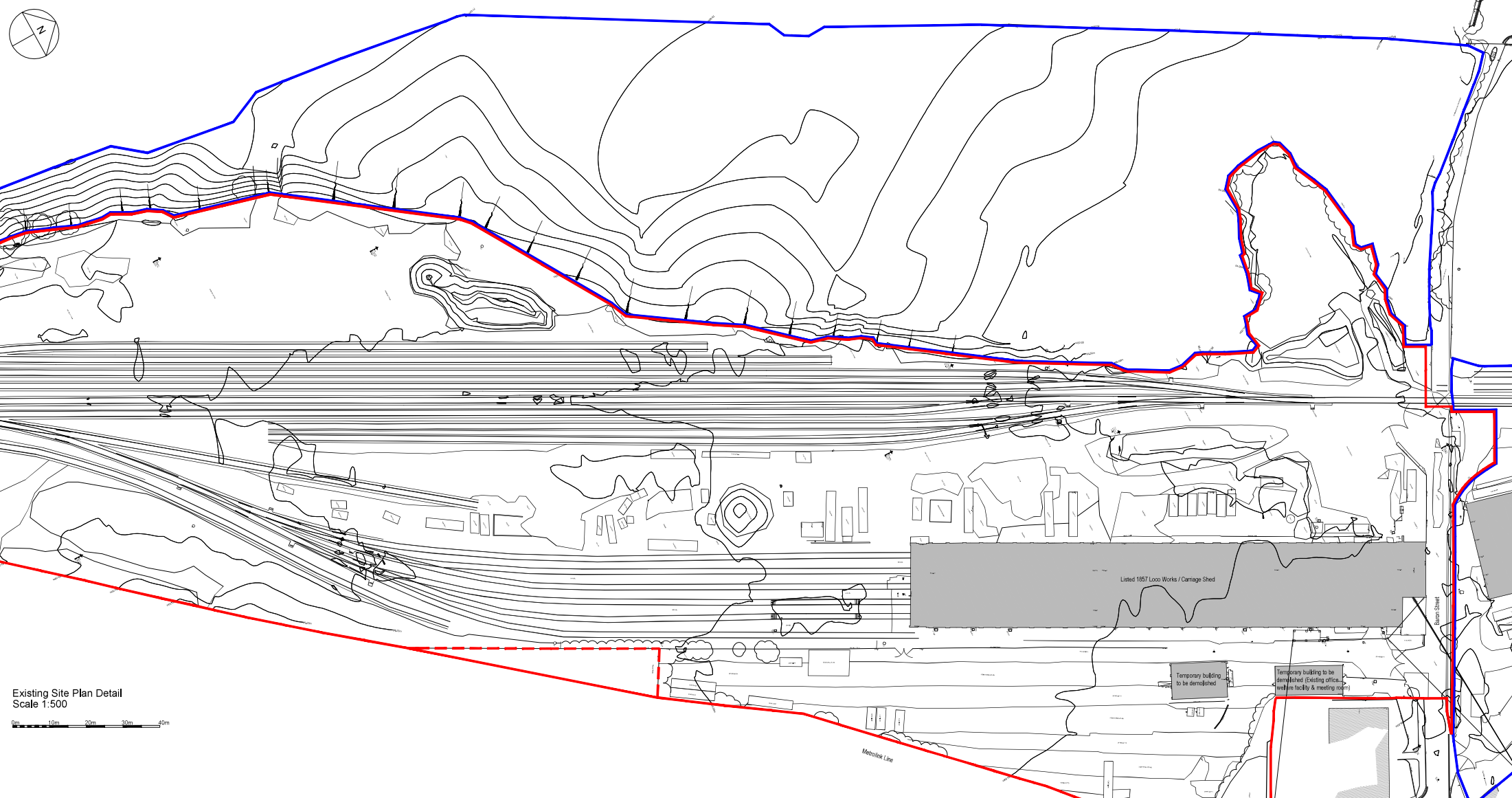
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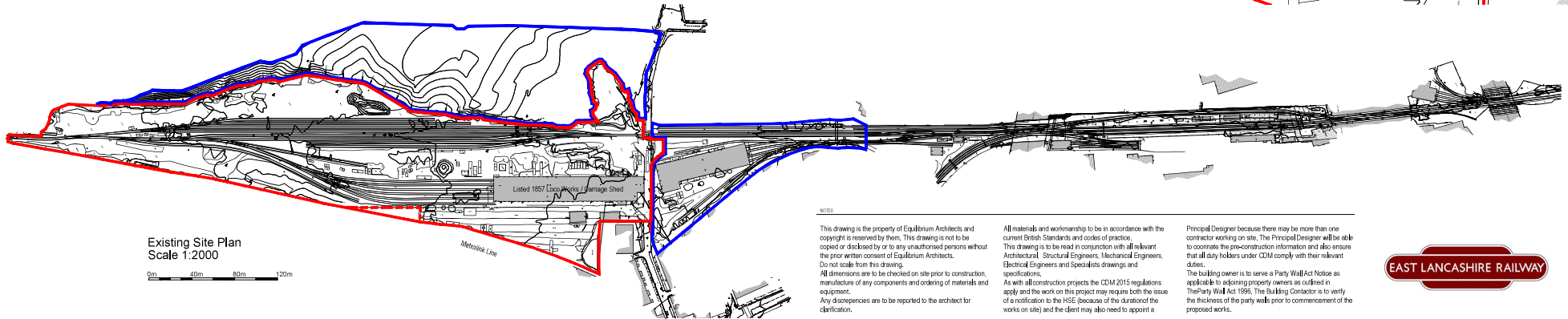
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Existing Site Plan Detail  
Scale 1:500



Existing Site Plan  
Scale 1:2000

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B Updated Fact / Building Works summary		13/01/2022	
A Updated Fact & Site Line to include the current works		16/12/2021	
REV	DESCRIPTION	CHECK	DATE
TASK			
East Lancs Railway			
PROJECT			
ELR Buckley Wells Masterplan			
DRAWING			
Existing Site Layout			
SCALE	DATE	DRAWN	CHECKED
1:500@A1	Sept 2018	PB	
DRAWING NO.	CAD REFERENCE NO.	REVISION	
C-0340-01		B	

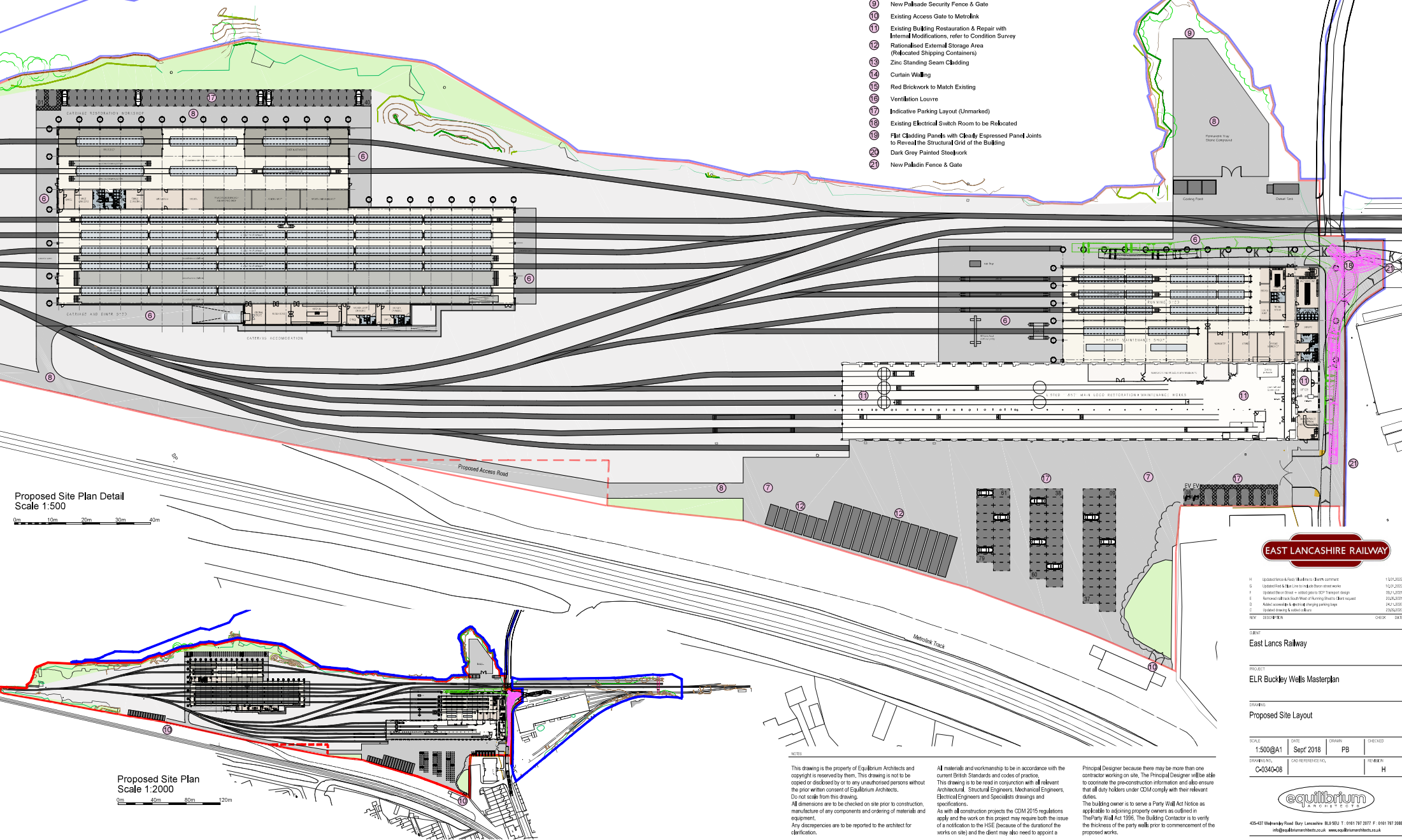
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info@equilibriumarchitects.co.uk www.equilibriumarchitects.co.uk







- Legend:**
- 1 Red Engineering Brickwork
  - 2 Steel Bulk-up cladding system, colour TBC
  - 3 New Steel Door on Rail Safety High Contrast Colour
  - 4 Polycarbonate Trapezoidal Translucent Rooflight
  - 5 Solar Panels
  - 6 Concrete Hardstanding
  - 7 Existing Ground Finish: Gravel & stone Setts
  - 8 New Ground Finish: Gravel
  - 9 New Palisade Security Fence & Gate
  - 10 Existing Access Gate to Metrolink
  - 11 Existing Building Restoration & Repair with Internal Modifications, refer to Condition Survey
  - 12 Rationalised External Storage Area (Relocated Shipping Containers)
  - 13 Zinc Standing Seam Cladding
  - 14 Curtain Walling
  - 15 Red Brickwork to Match Existing
  - 16 Ventilation Louvre
  - 17 Indicative Parking Layout (Unmarked)
  - 18 Existing Electrical Switch Room to be Relocated
  - 19 Flat Cladding Panels with Clearly Expressed Panel Joints to Reveal the Structural Grid of the Building
  - 20 Dark Grey Painted Steelwork
  - 21 New Palisade Fence & Gate



Proposed Site Plan Detail  
Scale 1:500

Proposed Site Plan  
Scale 1:2000

**EAST LANCASHIRE RAILWAY**

H	Updated terms & conditions (Business & contract)	13/01/2022
G	Updated terms & conditions (Business & contract)	13/01/2022
F	Updated terms & conditions (Business & contract)	13/01/2022
E	Updated terms & conditions (Business & contract)	13/01/2022
D	Updated terms & conditions (Business & contract)	13/01/2022
C	Updated terms & conditions (Business & contract)	13/01/2022
B	Updated terms & conditions (Business & contract)	13/01/2022
A	Updated terms & conditions (Business & contract)	13/01/2022

REV DESCRIPTION CHECK DATE

CLIENT  
East Lancs Railway

PROJECT  
ELR Buckley Wells Masterplan

DRAWING  
Proposed Site Layout

SCALE:	1:500@A1	DATE:	Sept 2018	DRAWN:	PB	CHECKED:	
DRAWING NO:	C-0340-08	DATE:		DRAWN:		CHECKED:	



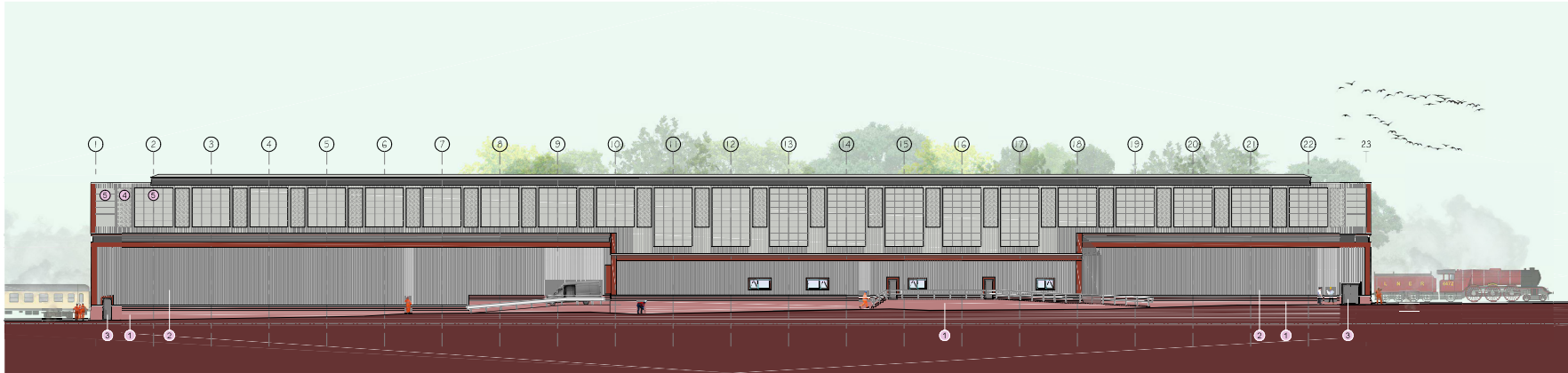
435-437 Widdowson Road, Bury, Lancashire, BL9 9EU T: 0161 787 2077 F: 0161 787 2088  
info@equilibriumarchitects.co.uk www.equilibriumarchitects.co.uk

**NOTES**

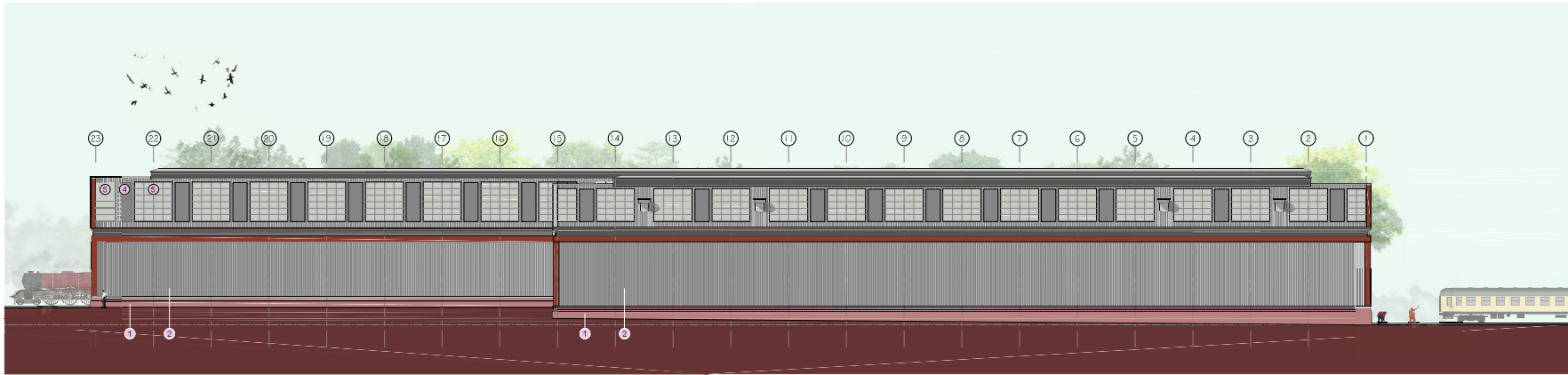
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Proposed South East Elevation  
Scale 1:200



Proposed North West Elevation  
Scale 1:200

0m 5m 10m 15m 20m

**Legend:**

- |   |   |   |
|---|---|---|
| ① Red Engineering Brickwork                       | ⑦ Existing Ground Finish: Gravel & stone Setts  | ⑬ Curtain Walling                                 |
| ② Steel Build-up cladding system, colour TBC      | ⑧ New Ground Finish: Gravel   | ⑭ Red Brickwork to Match Existing                 |
| ③ New Steel Door                                  | ⑨ Palisade Security Fence   | ⑮ Ventilation Louvre                              |
| ④ Polycarbonate Trapezoidal Translucent Rooflight | ⑩ Existing Access Gate to Metrolink   | ⑯ Indicative Parking Layout (Unmarked)            |
| ⑤ Solar Panels                                    | ⑪ Existing Building Restoration & Repair with Internal Modifications, refer to Condition Survey | ⑰ Existing Electrical Switch Room to be Relocated |
| ⑥ Concrete Hardstanding                           | ⑫ Relocated External Storage Area (Relocated Shipping Containers)                               |   |
|   | ⑬ Zinc Standing Seam Cladding   |   |

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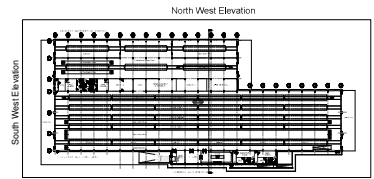
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Key Plan  
Scale 1:1000

EAST LANCASHIRE RAILWAY

DRAFT

A: 10/01/2018 12/01/2018  
REV: 01/01/2018 01/01/2018

East Lancs Railway

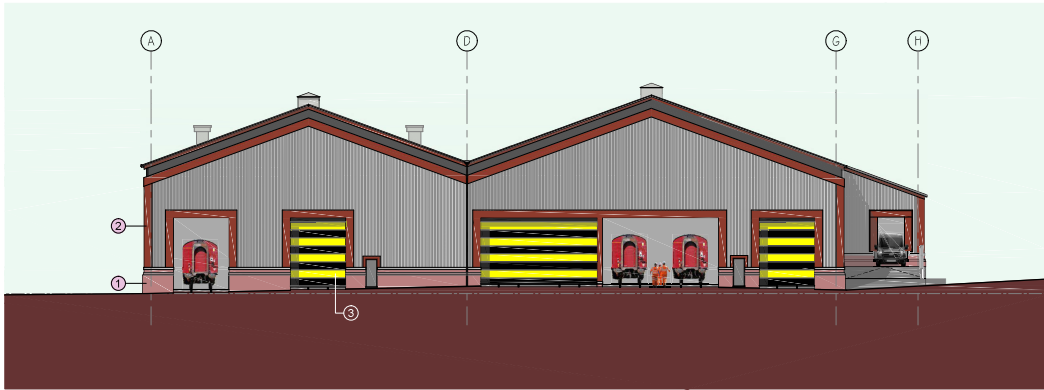
PROJECT  
ELR Buckley Wells Masterplan

DRAWING  
Proposed Carriage Shed  
South East & North West Elevations

SCALE	DATE	REVISION	BY
1:200@A1	Sep 2018	P8	04/04/2018
04/04/2018	04/04/2018	04/04/2018	04/04/2018

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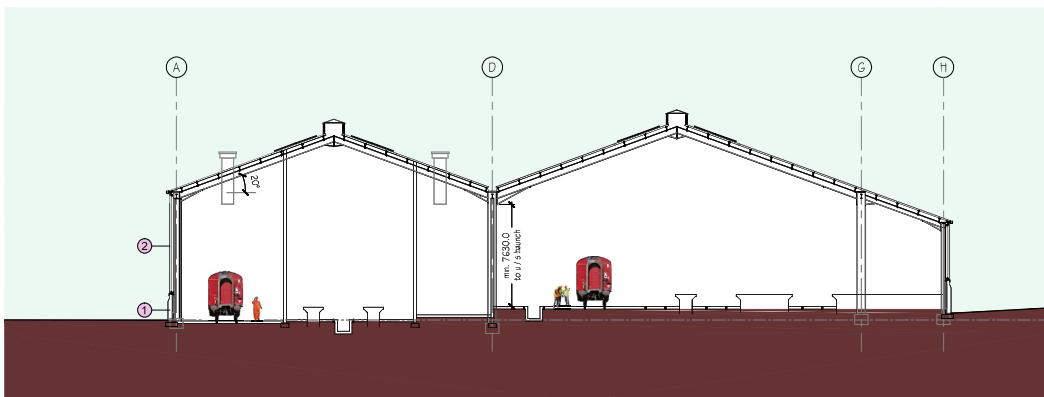
01547 444444 Road: Bay Lane, Bury, Lancashire, BL9 9JL T: 0161 787 2077 F: 0161 787 2088  
info@equilibriumarch.co.uk www.equilibriumarch.co.uk



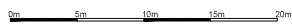
Proposed South West Elevation  
Scale 1:200



Proposed North East Elevation  
Scale 1:200



Proposed Section A-A  
Scale 1:200



- Legend:**
- ① Red Engineering Brickwork
  - ② Steel Build-up cladding system, colour TBC
  - ③ New Steel Door
  - ④ Polycarbonate Trapezoidal Translucent Rooflight
  - ⑤ Solar Panels
  - ⑥ Concrete Hardstanding
  - ⑦ Existing Ground Finish: Gravel & stone Setts
  - ⑧ New Ground Finish: Gravel
  - ⑨ Palisade Security Fence
  - ⑩ Existing Access Gate to Metrolink
  - ⑪ Existing Building Restoration & Repair with Internal Modifications, refer to Condition Survey
  - ⑫ Rationalised External Storage Area (Relocated Shipping Containers)
  - ⑬ Zinc Standing Seam Cladding
  - ⑭ Curtain Walling
  - ⑮ Red Brickwork to Match Existing
  - ⑯ Ventilation Louvre
  - ⑰ Indicative Parking Layout (Unmarked)
  - ⑱ Existing Electrical Switch Room to be Relocated

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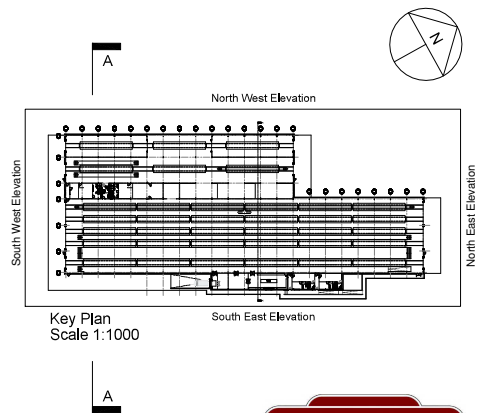
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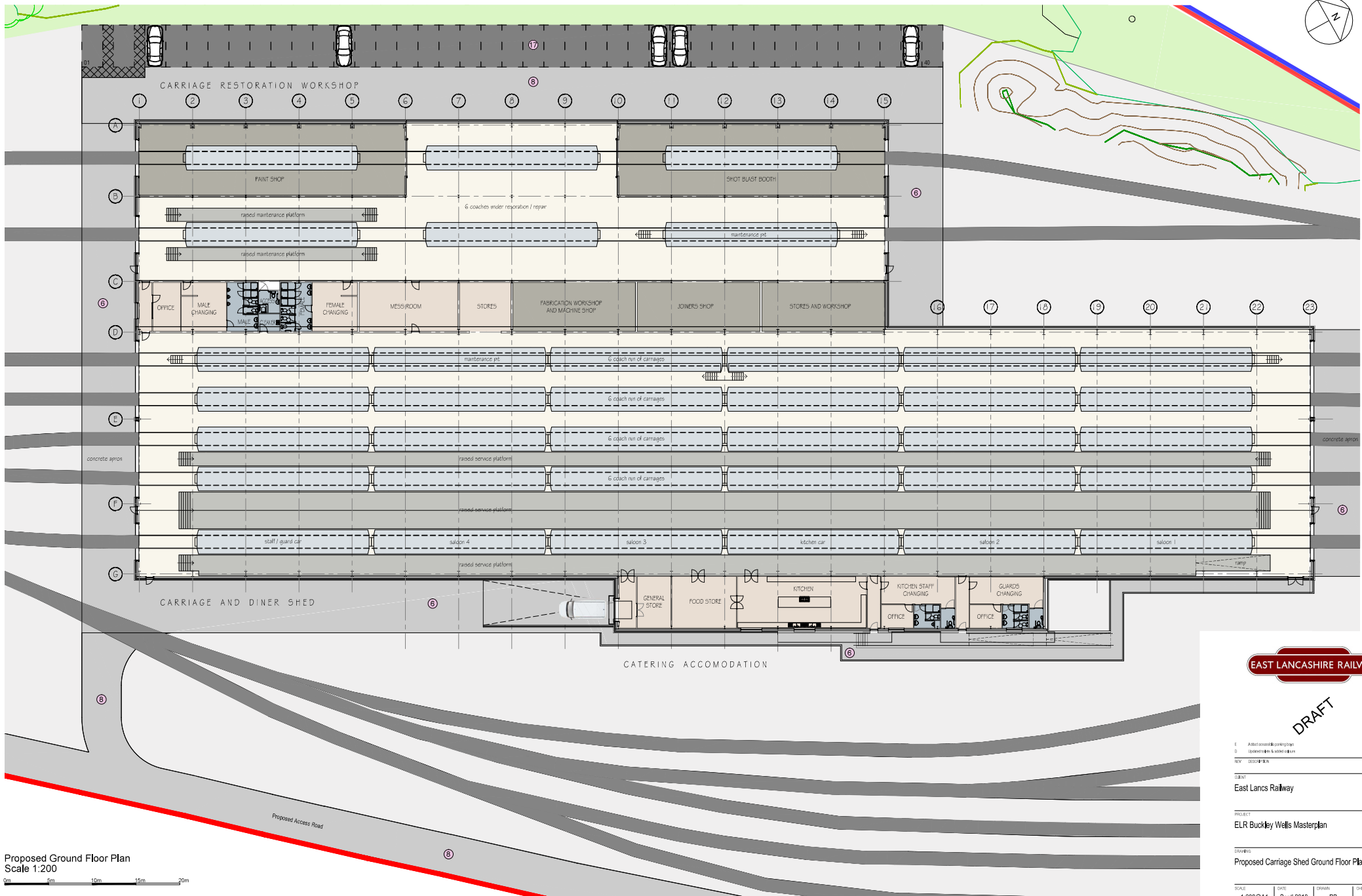
Key Plan  
Scale 1:1000

**EAST LANCASHIRE RAILWAY**

**DRAFT**

0	Author	09/09/2018
REV	DESCRIPTION	CHECK DATE
PROJECT		
East Lancs Railway		
PROJECT		
ELR Buckley Wells Masterplan		
DRAWING		
Proposed Carriage Shed		
South West, North East Elevations & Section A-A		
SCALE:	DATE:	CHECKED:
1:200@A1	Sept 2018	PB
DRAWING NO:	NO REFERENCE NO:	REVISION:
C-0340-101		D





Proposed Ground Floor Plan  
Scale 1:200

0m 5m 10m 15m 20m

- Legend:**
- ① Red Engineering Brickwork
  - ② Steel Bulk-up cladding system, colour TBC
  - ③ New Steel Door
  - ④ Polycarbonate Trapezoidal Translucent Rooflight
  - ⑤ Solar Panels
  - ⑥ Concrete Handstanding
  - ⑦ Existing Ground Finish: Gravel & stone Setts
  - ⑧ New Ground Finish: Gravel
  - ⑨ Palisade Security Fence
  - ⑩ Existing Access Gate to Metrolink
  - ⑪ Existing Building Restoration & Repair with Internal Modifications, refer to Condition Survey
  - ⑫ Rationalised External Storage Area (Relocated Shipping Containers)
  - ⑬ Zinc Standing Seam Cladding
  - ⑭ Curtain Walling
  - ⑮ Red Brickwork to Match Existing
  - ⑯ Ventilation Louvre
  - ⑰ Indicative Parking Layout (Unmarked)
  - ⑱ Existing Electrical Switch Room to be Relocated

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**EAST LANCASHIRE RAILWAY**

**DRAFT**

E	Author/consultant/permissions	P41/L2000
D	Updated/checked/checked	10/04/2020
REV	DESCRIPTION	CHECK DATE

**East Lancs Railway**

**PROJECT**  
**ELR Buckley Wells Masterplan**

**DRAWING**  
**Proposed Carriage Shed Ground Floor Plan**

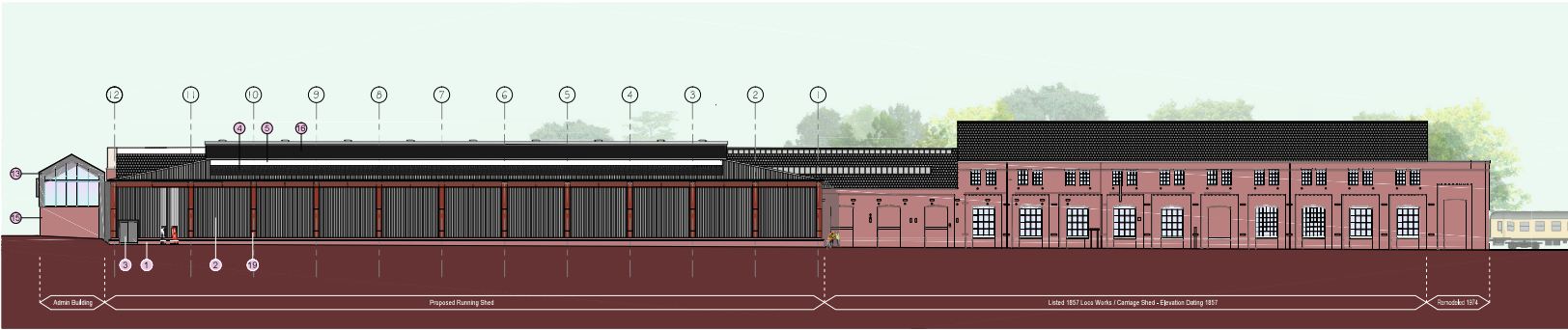
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1:200@A1	Sept 2018	PB	
DRAWING NO:	NO REFERENCE NO:	REVISION:	
C-0340-100		E	



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Proposed South East Elevation  
Scale 1:200



Proposed North West Elevation  
Scale 1:200

0m 5m 10m 15m 20m

- Legend:**
- 1 Red Engineering Brickwork
  - 2 Steel Bolt-Up cladding system, colour TBC
  - 3 New Steel Door on Rail Safety High Content Colour
  - 4 Polycarbonate Trapezoidal Translucent Rooflight
  - 5 Sider Panels
  - 6 Concrete Handstanding
  - 7 Existing Ground Finish: Gravel & stone Sets
  - 8 New Ground Finish: Gravel
  - 9 Palsade Security Fence
  - 10 Existing Access Gate to Metrolink
  - 11 Existing Building Restoration & Repair with Internal Modifications, refer to Condition Survey
  - 12 Relocated External Storage Area (Relocated Shipping Containers)
  - 13 Zinc Standing Seem Cladding
  - 14 Curtain Walling
  - 15 Red Brickwork to Match Existing
  - 16 Ventilation Louvre
  - 17 Indicative Parking Layout (Unmarked)
  - 18 Existing Electrical Switch Room to be Relocated
  - 19 Flat Cladding Panels with Clearly Expressed Panel Joints to Reveal the Structural Grid of the Building
  - 20 Dark Grey Painted Steelwork

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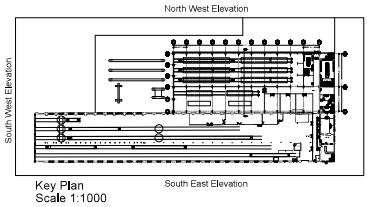
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Key Plan  
Scale 1:1000

EAST LANCASHIRE RAILWAY

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East Lancs Railway

PROJECT  
ELR Buckley Wells Masterplan

DRAWING			
Proposed Running Shed South East & North West Elevations			
SCALE	DATE	SHEET	DRAWING
1:200@A1	Sep 2018	P8	04/02/20
DRAWN BY	CHKD BY	DATE	BY
C404/2018			B

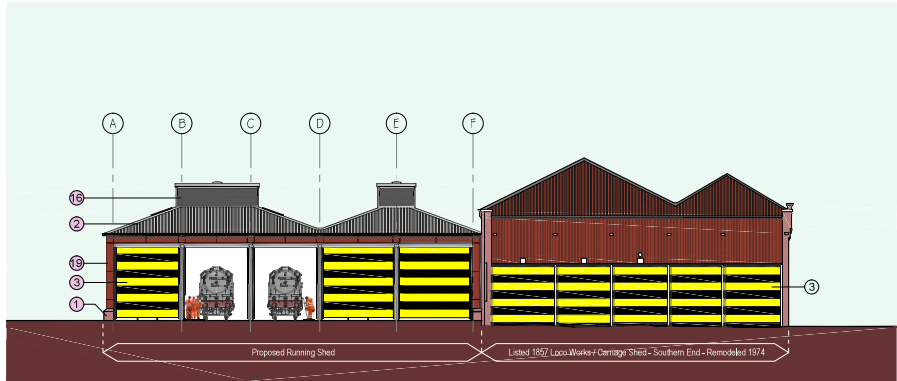
equilibrium  
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info@equilibriumarch.co.uk www.equilibriumarch.co.uk

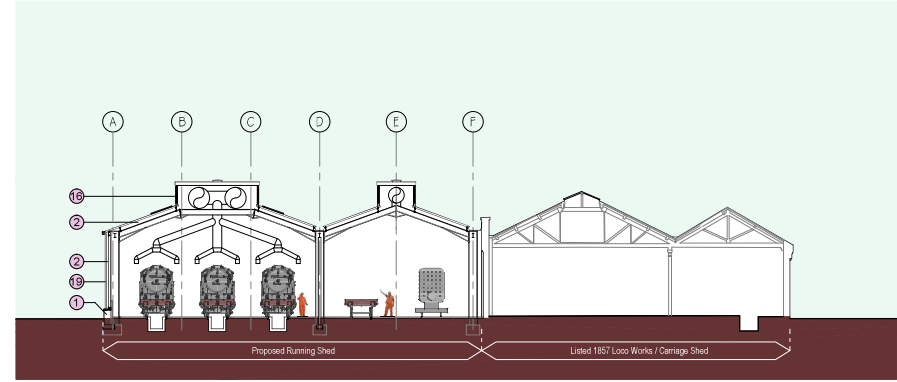




Proposed North East Elevation  
Scale 1:200



Proposed South West Elevation  
Scale 1:200



Proposed Section B-B  
Scale 1:200

- Legend:**
- 1 Red Engineering Brickwork
  - 2 Steel Bulk-up cladding system, colour TBC
  - 3 New Steel Door on Rail Safety High Contrast Colour
  - 4 Polycarbonate Trapezoidal Translucent Rooflight
  - 5 Solar Panels
  - 6 Concrete Hardstanding
  - 7 Existing Ground Finish: Gravel & stone Setts
  - 8 New Ground Finish: Gravel
  - 9 Palisade Security Fence
  - 10 Existing Access Gate to Metrolink
  - 11 Existing Building Restoration & Repair with Internal Modifications, refer to Condition Survey
  - 12 Rationalised External Storage Area (Relocated Shipping Containers)
  - 13 Zinc Standing Seam Cladding
  - 14 Curtain Walling
  - 15 Red Brickwork to Match Existing
  - 16 Ventilation Louvre
  - 17 Indicative Parking Layout (Unmarked)
  - 18 Existing Electrical Switch Room to be Relocated
  - 19 Flat Cladding Panels with Clearly Expressed Panel Joints to Reveal the Structural Grid of the Building
  - 20 Dark Grey Painted Steelwork

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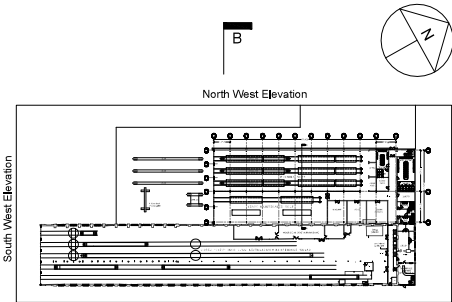
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Key Plan  
Scale 1:1000

**EAST LANCASHIRE RAILWAY**

B Updated South West Elevation 02/06/2021  
A Public Consultation 27/02/2020  
REV DESCRIPTION CHECK DATE

East Lancs Railway

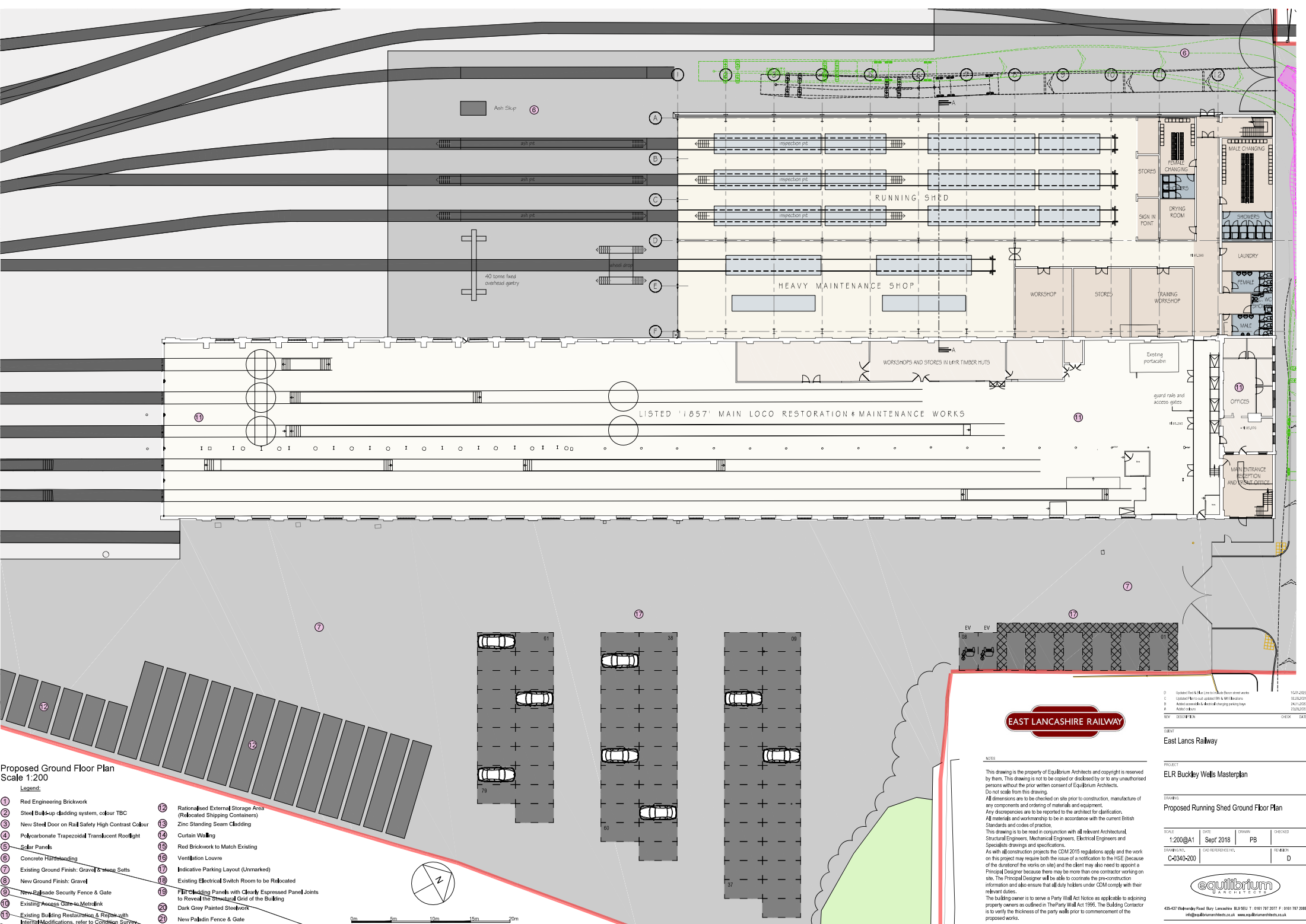
PROJECT  
ELR Buckley Wells Masterplan

DRAWING  
Proposed Running Shed  
South West, North East Elevations & Section B-B

SCALE: 1:200@A1 DATE: Sept 2018 DRAWN: PB CHECKED:  
DRAWING NO: C-0340-201 CAD REFERENCE: PCL REVISION: B

**equilibrium**  
ARCHITECTS

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info@equilibriumarchitects.co.uk www.equilibriumarchitects.co.uk



Proposed Ground Floor Plan  
Scale 1:200

- Legend:**
- 1 Red Engineering Brickwork
  - 2 Steel Bulk-up cladding system, colour TBC
  - 3 New Steel Door on Rail Safety High Contrast Colour
  - 4 Polycarbonate Trapezoidal Translucent Rooflight
  - 5 Solar Panels
  - 6 Concrete Hardstanding
  - 7 Existing Ground Finish: Gravel & Loose Setts
  - 8 New Ground Finish: Gravel
  - 9 New Rail-side Security Fence & Gate
  - 10 Existing Access Gate to MetroLink
  - 11 Existing Building Restraint & Repair with Internal Modifications, refer to Condition Survey
  - 12 Rationalised External Storage Area (Relocated Shipping Containers)
  - 13 Zinc Standing Seam Cladding
  - 14 Curtain Walling
  - 15 Red Brickwork to Match Existing
  - 16 Ventilation Louvre
  - 17 Indicative Parking Layout (Unmarked)
  - 18 Existing Electrical Switch Room to be Relocated
  - 19 Fibrecladding Panels with Clearly Expresed Panel Joints to Reveal the Structural Grid of the Building
  - 20 Dark Grey Painted Steelwork
  - 21 New Paladin Fence & Gate

**EAST LANCASHIRE RAILWAY**

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The building owner is to serve a Party Wall Notice as applicable to adjoining property owners as outlined in The Party Wall Act 1996. The Building Contractor is to verify the thickness of the party walls prior to commencement of the proposed works.

REV		DESCRIPTION	CHECK	DATE
1		Updated Red & Blue Line to show floor street works		15/07/2018
2		Updated North arrow and added 10m & 15m dimensions		16/07/2018
3		Added secondary & tertiary emergency entry points		16/07/2018
4		Added colours		16/07/2018

SCALE		DATE	DRAWN	CHECKED
1:200@A1		Sept 2018	PB	

DRAWING NO.		NO REFERENCE NO.	REVISION
C-0340-200			D

**equilibrium**  
ARCHITECTS

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