

# Northern Gateway Development Framework (JPA1.1)

Supplementary Planning Document

March 2025



ROCHDALE  
BOROUGH COUNCIL

Bury  
Council

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**Date of issue**  
March 2025

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# Foreword

The recently approved Places for Everyone Joint Development Plan designates the Northern Gateway site for a primarily employment-focused development, with about 1.2 million square metres of space. This project is expected to create around 20,000 new jobs, benefitting residents of Bury, Rochdale, Greater Manchester, and surrounding areas.

The site is a key part of the Atom Valley Mayoral Development Zone, which was created to promote high-tech industries, innovations, and economic growth. Its goal is to position Greater Manchester as a leader in cutting-edge industries.

The Northern Gateway offers a truly transformative opportunity to reshape the local economy and create a lively, accessible, and dynamic business hub for various industries, including specialised areas for advanced manufacturing. It will feature a variety of building sizes and attract everything from new start-ups and growing local businesses to international companies looking to invest.

Northern Gateway is more than just a business hub. It offers the chance to build a new community, with new homes, green spaces, and transport infrastructure that will provide lasting benefits to the people of Bury, Rochdale, and beyond. This development aims to create a lasting legacy for future generations.

We (Bury Council and Rochdale Council) are proud to present the Northern Gateway Development Framework, which outlines a clear plan, vision, and objectives for the site. This framework will set the guidelines for evaluating future planning applications and identify the essential infrastructure needed to support the site's long-term development.

"This is a true game-changer, a once in a lifetime opportunity to bring jobs and investment to our part of the world. It will play a crucial role in building prosperous communities in Bury and the wider area for generations to come".

**Cllr Eamonn O'Brien, Leader of Bury Council**

"The Northern Gateway, as part of the wider Atom Valley, presents us with a transformative opportunity to rebalance the Greater Manchester economy and bring thousands of high-quality jobs, improved transport links to surrounding areas and increased prosperity to the Rochdale borough and beyond."

**Cllr Neil Emmott, Leader of Rochdale Council**



**Cllr Eamonn O'Brien, Leader  
of Bury Council**



**Cllr Neil Emmott, Leader of  
Rochdale Council**



# > Northern Gateway





# Introduction

This Northern Gateway Development Framework (NGDF) relates to the Heywood/ Pilsworth (Northern Gateway) site



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# 01. Introduction

This Northern Gateway Development Framework (NGDF) relates to the Heywood/ Pilsworth (Northern Gateway) site, that is allocated for employment-led development under Policy JPA1.1 of the recently adopted Places for Everyone Joint Development Plan (PfE) and forms part of the wider ‘Northern Gateway’ development opportunity.

1.1 Northern Gateway comprises two PfE allocations within the wider North-East Growth corridor which straddle the districts of Bury and Rochdale:

- Heywood/Pilsworth – Policy JPA1.1, and
- Simister/Bowlee – Policy JPA1.2.

1.2 This NGDF relates solely to the JPA1.1 allocated site (hereafter known as ‘the site’). A separate development framework is to be prepared for the JPA1.2 allocation.

1.3 The role of the NGDF is to inform the preparation of the Design Code(s) and Infrastructure Phasing and Delivery Strategy (IPDS) for the site and to effectively bridge the gap between the allocation of the site in PfE and the subsequent detail of the development that will emerge through planning applications on the site.

1.4 The preparation of the NGDF and its supporting evidence has enabled key issues to be identified and addressed to the satisfaction of all parties at an early stage in the planning process. Fundamentally, the Development Framework establishes the parameters against which future planning applications will be considered and identifies the necessary infrastructure required to support the development of the site.

1.5 The NGDF has been prepared as a Supplementary Planning Document (SPD) and once adopted it will be a material consideration in the determination of all planning applications relating to the site.

1.6 The site straddles the administrative boundary of Bury and Rochdale and is positioned at a strategically important intersection around the M60, M62 and M66 motorways. It represents a highly accessible opportunity for growth in Greater Manchester with wider benefits on a regional and national level. It will enable the delivery of a large, nationally significant employment opportunity to attract high quality businesses and investment on the M62 corridor coupled with a significant number of new homes and supporting services and facilities.



1.7 A central theme of PfE's spatial strategy is to deliver inclusive growth across the city region complemented by a key aim to significantly boost the competitiveness of the northern parts of Greater Manchester. The scale and location of the site will help to deliver these fundamental objectives. It will help to rebalance the Greater Manchester economy, ensure PfE plays its part in driving growth within the north of England and significantly improve Greater Manchester's competitiveness both nationally and internationally.

1.8 This NGDF has been prepared jointly by Bury and Rochdale Councils (hereafter collectively referred to as 'the councils'), with technical input from the Northern Gateway Development Vehicle LLP (NGDV) as the main site promoter and input from other landowners / stakeholders, including National Highways and Transport for Greater Manchester (TfGM). This engagement has helped to identify and understand expectations and key issues around the delivery of the site during the early stages of preparing the NGDF and all parties are committed to securing the comprehensive development of the site.

## The Purpose of the NGDF

### Places for Everyone Context

1.9 The Places for Everyone Plan (PfE) is a joint development plan document of nine Greater Manchester authorities (excluding Stockport) which was adopted in March 2024.

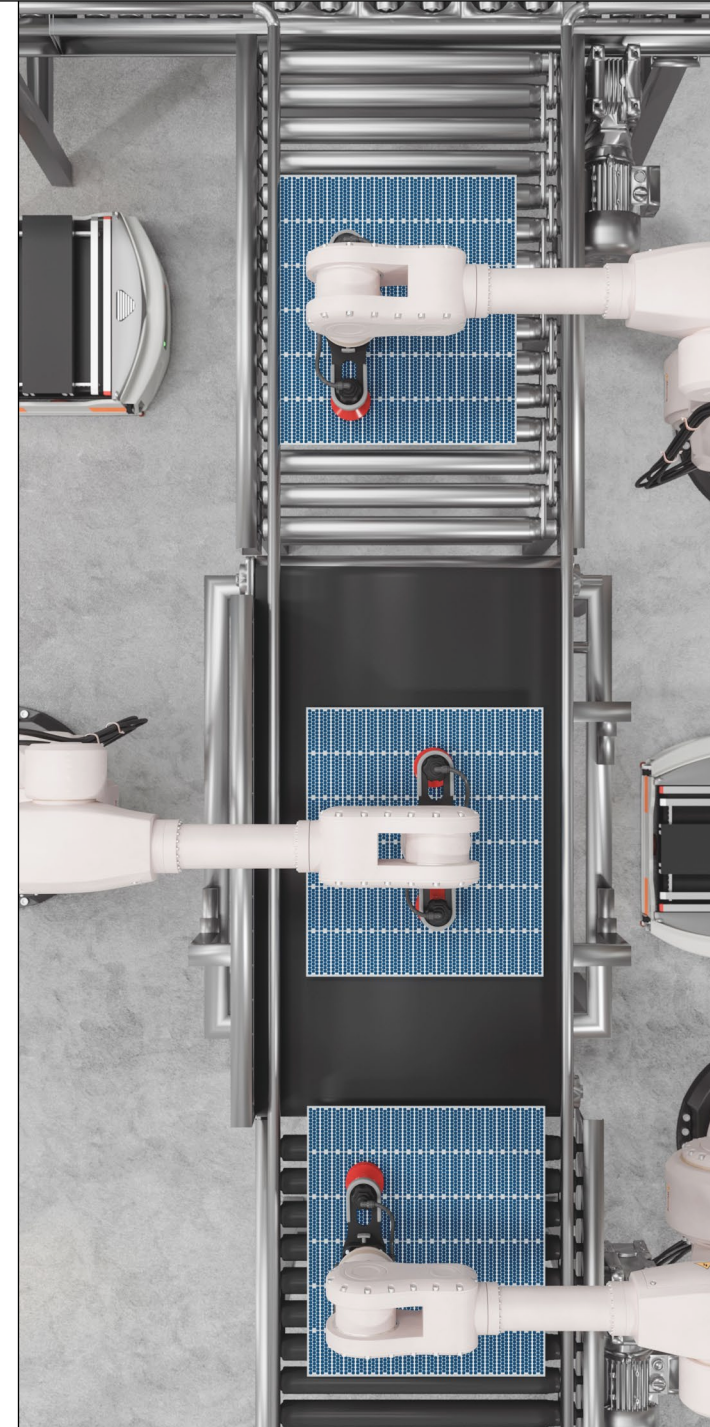
1.10 It is a long term plan for jobs, new homes and sustainable growth which allocates large strategic sites for employment and housing. This includes the employment-led allocation at Northern Gateway (Heywood/Pilsworth) (PfE Policy JPA1.1).

1.11 PfE Policy JPA1.1 includes the following requirement:

"Development at this allocation will be required to be in accordance with a comprehensive masterplan, design code and infrastructure phasing and delivery strategy, in line with Policy JP-D1 'Infrastructure Implementation', that has been agreed with the local planning authorities."

1.12 The NGDF supplements PfE Policy JPA 1.1 and sets out a comprehensive Development Framework and high level design principles. Design Code(s) are expected to come forward at planning application stage.

1.13 A comprehensive Infrastructure Phasing and Delivery Strategy (IPDS) for the site is being prepared in accordance with the PfE Policy JPA1.1 and JP-D1 and should be read alongside the NGDF SPD. The IPDS will remain a 'living document' and will be subject to regular updates and amendments outside of the NGDF SPD process in order to be responsive to constraints and opportunities, and the availability of funding, as the site is brought forward.





## NGDF Role and Objectives

**1.14** This NGDF will establish the key principles for the development of the site and establish a framework against which future planning applications on the site will be considered. It will also help Bury and Rochdale Councils to ensure that the ambitions for the site are realised and the benefits to the communities of Bury and Rochdale are maximised.

**1.15** This NGDF will:

- Set out the councils' vision and strategic objectives for the site shaped by public consultation and stakeholder collaboration
- Facilitate a coordinated and comprehensive masterplanning approach, to be implemented consistently across multiple planning applications
- Establish key development requirements that all planning applications relating to land within the site are expected to adhere to
- Provide an overarching spatial Development Framework to communicate development and design parameters, including land use, access, movement and green infrastructure
- Provide design principles and guidance, to inform the more detailed design considerations and approaches needed to deliver the Northern Gateway vision, and
- Describe the proposed approach towards delivery of physical infrastructure.

**1.16** This NGDF has been produced in accordance with the requirements of legislation and the National Planning Policy Framework (NPPF).

**1.17** This NGDF establishes a clear framework for development at the site and will be a material consideration in the determination of all future planning applications within the allocation boundary. In accordance with the adopted PfE, planning applications will be required to align with the Development Framework and supporting guidance set out in this NGDF including delivery of necessary and related infrastructure in a coordinated and timely manner.

## NGDF Structure

**1.18** This NGDF has been structured to reflect the sequence of key stages in its production, as follows:

- **Chapter 2** – Strategic Policy Context: In preparing the NGDF, consideration has been given to alignment of the site with national, regional and local policy.
- **Chapter 3** – Vision and Strategic Objectives: The overarching vision for the site and the strategic objectives which will help achieve the vision.
- **Chapter 4** – Local and Strategic Outcomes: An outline of the economic, social and environmental benefits the site will deliver.
- **Chapter 5** – Understanding the Site and Surroundings: Identifies the site within its wider strategic setting and sets out the site conditions. An overview of the planning policy context and planning history. Concluding with site constraints and opportunities.
- **Chapter 6** – Key Development Principles: An introduction to the Development Framework for the site and presentation of the key design principles.
- **Chapter 7** – Design Principles and Character Areas: Presentation of strategic design principles for a series of character areas within the site. This chapter presents a high-level framework to which future planning applications for individual phases must accord.
- **Chapter 8** – Sustainability: Sets out the expectations and likely benefits from the site with respect to net zero, carbon, energy, resource efficiency and climate resilience which, collectively, would make a significant contribution to climate change mitigation and address the impacts of climate change.
- **Chapter 9** – Phasing, Infrastructure and Delivery: Analysis of existing infrastructure including means of access, local and strategic highway network, community facilities and utilities / services. This chapter also includes details of how the site will be delivered in the future and roles and responsibilities.
- **Chapter 10** – Monitoring and Review: An outline for the ongoing strategy for engagement with key stakeholders in line with the requirements of PfE Policy JP-D1 and commitments to monitoring and review.



## SITE SIZE

634<sub>Ha</sub>

## LOCAL COMPARISON

560<sub>Ha</sub>      260<sub>Ha</sub>

MANCHESTER  
AIRPORT      HEATON  
PARK

(1.13x BIGGER)      (2.44x BIGGER)

## UK COMPARISON

10.9<sub>Ha</sub>      289<sub>Ha</sub>

02  
ARENA      CITY OF  
LONDON

(33.7x BIGGER)      (2.19x BIGGER)

## Overview of the Site

1.19 The site covers an area of around 634 hectares (1,567 acres) and is positioned at a strategically important intersection directly to the east of the M66, directly to the north west of the M62 and north of the M60. The site straddles the administrative boundary of both Bury and Rochdale. 327 hectares of the site are within Bury and 307 hectares are within Rochdale.

1.20 The site is situated to the east of Bury and to the south of Heywood and is in close proximity to neighbouring town centres. From the centre of the site (approx. Birch Business Park), as the crow flies, the distances are as follows; Heywood (2.8km), Middleton (3.7km), Whitefield (4.2km), Bury (4.4km) and Rochdale (7.3km).

1.21 The site consists primarily of agricultural land. There are a number of farms within the site as well as Birch Business Park, Birch Motorway Services and Pike Fold Golf Course. Whittle Brook, running south east to north west through the site creates a clear division between the south western and central portion of the site. Moss Hall Road (changing to Whittle Lane to the south east) creates another clear division between the central and north eastern portion of the site.

1.22 The site is allocated for substantial employment-led development. This will be supported by new communities within the site as well as at Simister/Bowlee (JPA 1.2) which have transformational potential in enabling new housing, community facilities and new transport infrastructure to come forward in what is currently an area with significant pockets of high deprivation, low skills and worklessness.

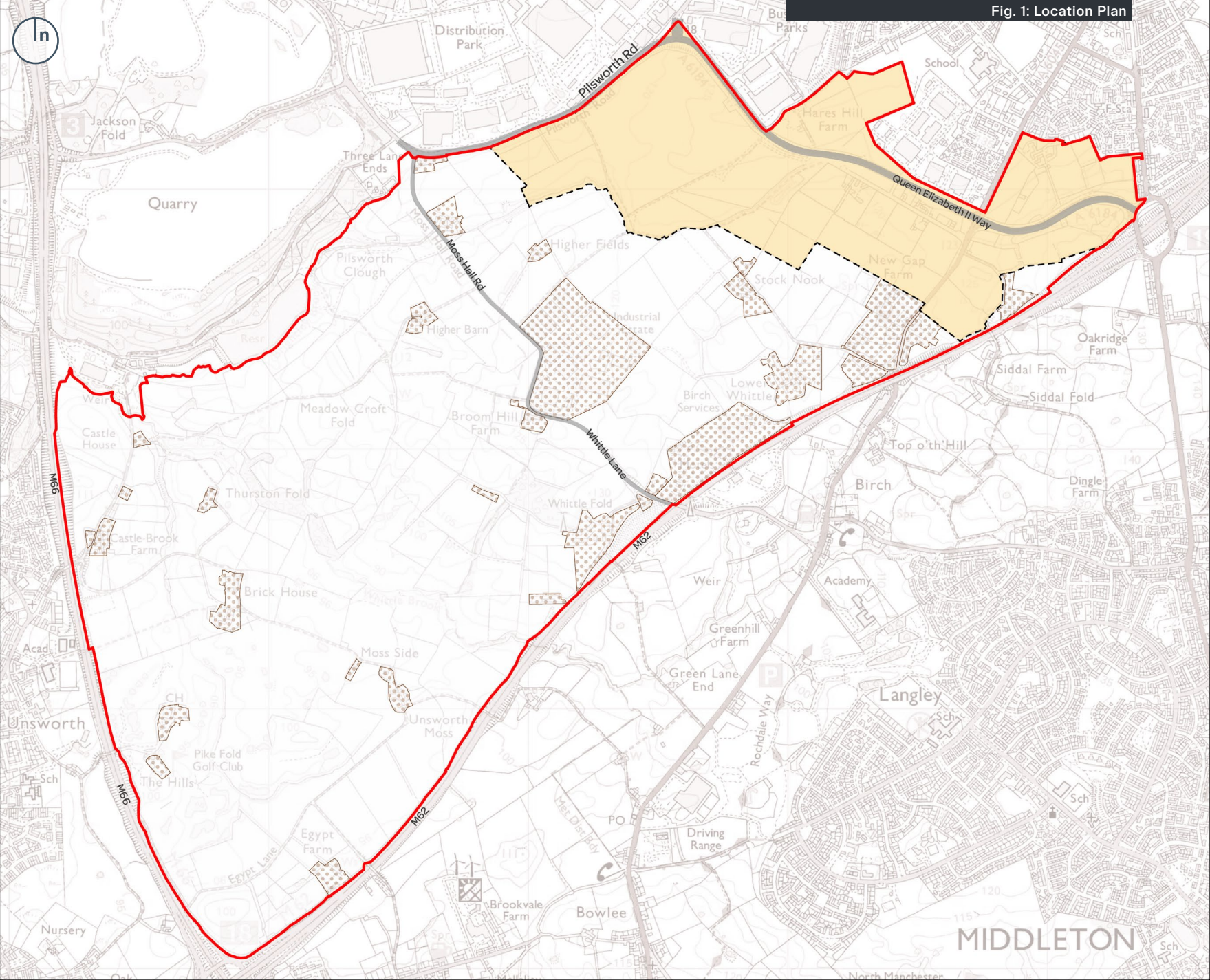
1.23 To be successful and sustainable, the employment and housing opportunities need to be accessible by a range of transport modes and be linked directly to existing and new communities in the surrounding area via new recreational routes and corridors of green infrastructure which in turn provide an attractive setting for development. Outside of the motorway network, much of the area proposed for development is currently served by an inadequate transport network and this will require substantial investment to improve connectivity, potentially including investment in rapid transit. The prospective residents will require new community facilities, and these will be provided in accessible locations within walking distance of homes.

1.24 Full delivery of the site is expected to extend beyond the PfE plan period (i.e. beyond 2039) and will need to incorporate extensive supporting infrastructure.

1.25 Part of the site is already being delivered in the form of the South Heywood development; a large mixed-use development which was approved in March 2020<sup>1</sup> and is currently under construction. This comprises the South Heywood employment area, 1,000 new homes and includes the recently built 2.2km link road, Queen Elizabeth II Way, which joins Junction 19 of the M62 motorway to existing employment areas off Pilsworth Road.

Fig. 1: Location Plan

- Allocation Boundary
- South Heywood Application Area
- Existing Development
- Existing Road







## Boundaries

1.26 North west - The north western boundary of the site is defined by wooded vegetation and public footpaths along Hollins Brook, running adjacent to a series of fishing ponds and a reservoir. Beyond is Pilsworth quarry/landfill site and Heywood Distribution Park.

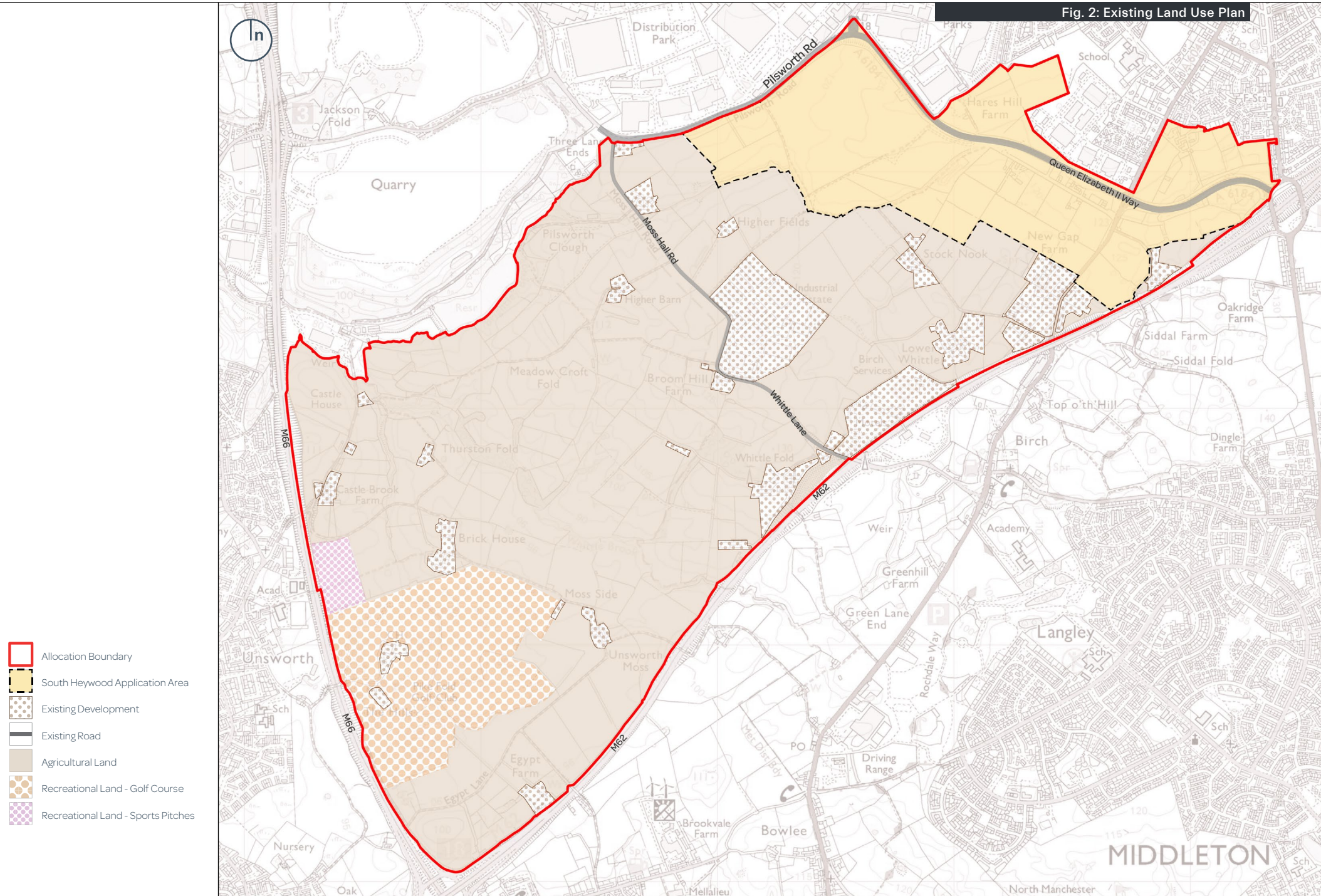
1.27 North east - The north eastern boundary is defined by the extent of the South Heywood Masterplan; beyond which is Heywood Distribution Park and existing residential development in Heywood.

1.28 South east - The M62 corridor, including Birch Services, runs along the entirety of the south eastern boundary. Beyond the M62 there is agricultural land, some of which forms part of the other site within the Northern Gateway allocation – known as Simister/Bowlee (Policy JP Allocation 1.2) – which is allocated for a 1,550 dwelling residential development, and the existing settlement of Simister.

1.29 West - The M66 corridor defines the western boundary, beyond which there is open space and existing residential communities of Unsworth and Whitefield within Bury.



Fig. 2: Existing Land Use Plan



## NEW EMPLOYMENT FLOORSPACE

 1.2m<sup>sqm</sup>

## NEW RESIDENTIAL HOMES

 1,200

## GROSS DIRECT FTE JOBS CREATED

 21,455

## NET ADDITIONAL CONSTRUCTION JOBS

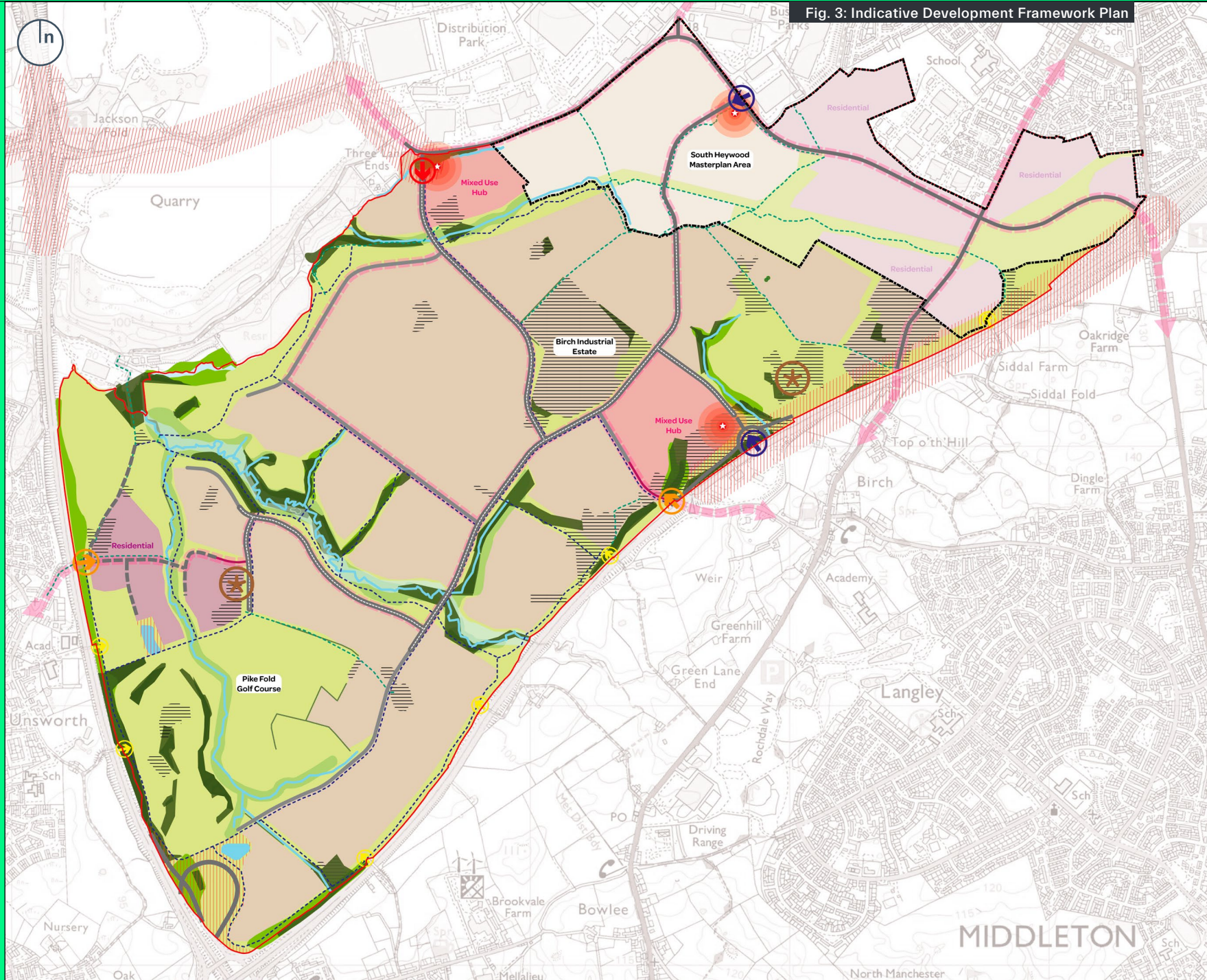
 15,600

## Northern Gateway Development Framework

1.30 Consistent with the Places for Everyone allocation, development at Northern Gateway, informed by the Northern Gateway Development Framework (NGDF), will deliver an employment-led, mixed-use development, comprising approximately 1,200,000 sqm of B2 / B8 floorspace (with around 935,000 sqm being delivered within the plan period), approximately 1,200 new homes, a new primary school and an appropriate range of supporting and ancillary services and facilities, such as a new local centre, hotel, leisure and conference facilities, as well as new and improved sustainable transport, active travel and highways infrastructure and multifunctional green and blue infrastructure.



- Allocation boundary
- Potential developable area
- Existing built development
- South Heywood Application Area
- Green open space
- Woodland
- Woodland Riparian Corridor
- Woodland/Priority Habitat
- Watercourse
- Listed Building (Grade II)
- Indicative Highway Improvement Area
- Proposed Access Point (all traffic)
- Proposed Access Point (restricted traffic)
- Potential Access Point (all traffic)
- Proposed Access Point (pedestrian / cycle access)
- Primary Road (all traffic)
- Secondary Road (all traffic)
- Residential Road (no HGV)
- Bus Road (bus only)
- Retained Footpath
- Relocated/New Shared Path
- Employment uses
- Residential uses
- Mixed-use
- Gateway location
- Bus enabled route



# Strategic Policy Context

Places for Everyone (PfE) is a long-term joint development plan for nine Greater Manchester authorities for jobs, new homes, and sustainable growth.



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## 02. Strategic Policy Context

### Planning Policy Context

#### Places for Everyone Joint Development Plan

2.1 Places for Everyone (PfE) is a long-term joint development plan for nine Greater Manchester authorities (Bolton, Bury, Manchester, Oldham, Rochdale, Salford, Tameside, Trafford and Wigan) for jobs, new homes, and sustainable growth. It has been produced by the Greater Manchester Combined Authority (GMCA) on behalf of the nine authorities and became part of the statutory Development Plan on 21st March 2024.

2.2 The PfE Joint Development Plan:

- sets out how the nine authorities should develop over the plan period to 2039
- identifies the amount of new development that will come forward across the nine authorities, in terms of housing, offices, and industry and warehousing, and the main areas in which this will be focused
- supports the delivery of key infrastructure, such as transport and utilities
- protects the important environmental assets across the city region
- allocates sites for employment and housing outside of the existing urban area, and
- defines a new Green Belt boundary for Greater Manchester.

2.3 PfE highlights the opportunity for growth along the 'North-East Growth Corridor' (PfE Policy JP-Strat 7), focused on the M62 corridor from Junction 18 to Junction 21, extending across parts of Bury, Rochdale and Oldham. Three sites have been allocated as part of the North-East Growth Corridor:

- Northern Gateway Heywood / Pilsworth in Rochdale and Bury (JPA1.1);
- Northern Gateway Simister / Bowlee in Rochdale and Bury (JPA1.2); and
- Stakehill, in Rochdale and Oldham (JPA2)

2.4 As set out below, the Atom Valley Mayoral Development Zone (MDZ) has been established to realise the potential of the North-East Growth Corridor.

2.5 PfE identifies the need for investment in new public transport infrastructure and routes to connect Bury and Rochdale Town Centres with the Northern Gateway sites and to Manchester City Centre. A potential Bus Rapid Transit (BRT) corridor linking Manchester city centre and Rochdale via Heywood Old Road/Manchester Road is identified in PfE as a supporting transport intervention that could further improve the sustainability of the Northern Gateway allocation.

### Policy JP Allocation 1.1: Heywood / Pilsworth (Northern Gateway)

2.6 JPA1.1 Heywood / Pilsworth has been identified as a large, nationally significant location for new employment-led development within the Northern Gateway opportunity area between Bury and Rochdale.

2.7 PFE Policy JP Allocation 1.1 states:

Development at this allocation will be required to:

1. Be in accordance with a comprehensive masterplan, design code and infrastructure phasing and delivery strategy, in line with Policy JP-D1 'Infrastructure Implementation', that has been agreed with the local planning authorities;
2. Be of sufficient scale and quality to enable a significant rebalance in economic growth within the sub-region by boosting the competitiveness of the north of the conurbation and should;
3. Deliver a total of around 1,200,000 sqm of industrial and warehousing space (with around 935,000 sqm being delivered within the plan period). This should comprise a mix of high quality employment premises in an attractive business park setting in order to appeal to a wide range of business sectors including the development of an Advanced Manufacturing Park;
4. Deliver around 1,000 additional homes along with a new primary school in the eastern part of the allocation to support the early delivery of the infrastructure and provide a buffer between existing housing and the new employment development;
5. Deliver around 200 new homes, which includes an appropriate mix of house types and sizes and the provision of plots for custom and self-build housing (subject to local demand, having regard to Bury's self-build register and other relevant evidence), in the west of the allocation off Castle Road ensuring that an appropriate buffer is incorporated to separate this part of the allocation from the wider employment area and that appropriate highways measures are in place to prevent the use of residential roads by traffic associated with the wider employment area;
6. Deliver an appropriate range of supporting and ancillary services and facilities, such as a new local centre, hotel, leisure and conference facilities. These should be in accessible locations and of a genuinely ancillary scale that is appropriate to the main employment use of the allocation;
7. Make provision for new and improved sustainable transport and highways infrastructure having regard to the indicative transport interventions set out in Appendix D in accordance with Policy JP-C8;
8. Make provision for affordable housing in accordance with local planning policy requirements;
9. Make financial contributions for offsite additional primary and secondary school provision to meet needs generated by the development in accordance with policy JP-P5;
10. Ensure the design and layout allows for effective integration with surrounding communities, including active travel links and connections to local services, employment opportunities and over the M62 to proposed new development at Simister/Bowlee (JPA1.2);
11. Retain and enhance existing recreation facilities (including Unsworth Academy playing fields and Pike Fold Golf Course) or, where necessary, make provision for replacement facilities that are equivalent or better in terms of quantity and quality and in a suitable location;
12. Make provision for new, high quality, publicly accessible multi-functional green and blue infrastructure including the integration and enhancement of existing features such as Brightley Brook, Whittle Brook and Castle Brook;
13. Make provision for compensatory improvements to the environmental quality and accessibility of remaining Green Belt in the vicinity of the site in accordance with Policy JP-G2;
14. Strengthen the boundary of the Green Belt to the north of the site around Pilsworth Cottages, Brightley Brook and Pilsworth Fisheries such that they will comprise physical features that are readily recognisable and likely to be permanent;
15. Ensure that any development is safe from and mitigates for potential flood risk from all sources including Whittle Brook, Castle Brook and Brightley Brook and does not increase the flood risk elsewhere. The delivery of the allocation should be guided by an appropriate flood risk and drainage strategy which ensures co-ordination between phases of development;
16. Provide an appropriate buffer between the development and the motorway/local road network where required to serve multiple functions including air quality, noise, visual mitigation and high-quality landscaping;

17. Take appropriate account of relevant heritage assets, and their settings, including Brick Farmhouse and Lower Whittle Farmhouse Grade II Listed Buildings in accordance with Policy JP-P2;
18. Carry out a detailed assessment and evaluation of known and potential archaeological sites including Meadow Croft Farm, historic landscape features and built heritage assets, to establish specific requirements for the protection and enhancement of significant heritage assets;
19. Consider the extraction of any viable mineral resources within Mineral Safeguarding Areas, in accordance with Policy 8 of the Greater Manchester Joint Minerals Development Plan (or any relevant policies in subsequent minerals plans); and
20. Undertake hydrological and ground investigations to determine the extent and quality of any peat identified in the south-western part of the site to inform the potential for restoration and the comprehensive masterplanning of the site which should ensure that the loss or deterioration of any irreplaceable habitat is avoided.

### Other PfE Policies

2.8 PfE also sets out a range of other policies relating to development management and technical / environmental matters which have also been taken into account in preparing this SPD.

## Local Plan Policies

2.9 In addition to PfE, the adopted Local Plans for Bury and Rochdale also set out a range of policies relating to development management and technical / environmental matters which have also been taken into account in preparing this SPD. These include:

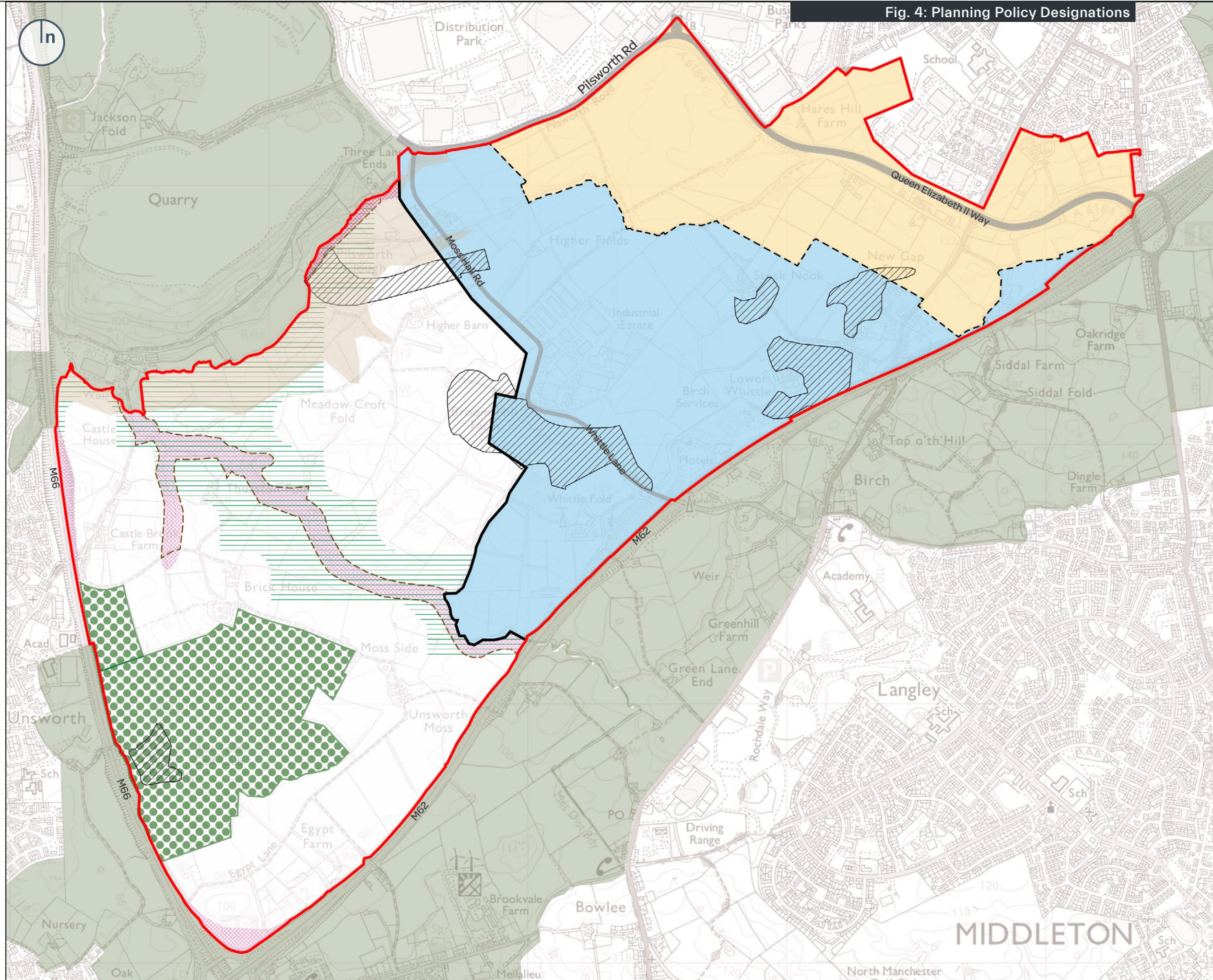
- Saved Policies of the Bury Unitary Development Plan (adopted 1997)
- The Rochdale Core Strategy (adopted 2016)
- Saved Policies of the Rochdale Unitary Development Plan (adopted 2006)
- The Greater Manchester Joint Waste Development Plan (adopted 2012)
- The Greater Manchester Joint Minerals Development Plan (adopted 2013)

2.10 Both councils have commenced preparation of new Local Plans that will support PfE and establish more detailed policy relating to development management and technical / environmental considerations.



Fig. 4: Planning Policy Designations

-  Allocation Boundary
-  South Heywood Application Area
-  Bury & Rochdale Boundary
-  PFE Green Belt
-  Protected Recreation
-  UDP River Valleys
-  UDP Wildlife Links & Corridors
-  Area of Search: Sand and Sandstone
-  PFE Green Infrastructure
-  PFE Urban Fringe Farmland
-  PFE Incised Urban Fringe Valleys
-  PFE Mosslands and Lowland Farmland
-  Existing Roads



## National Planning Policy Framework & Planning Practice Guidance

2.11 The National Planning Policy Framework (December 2024) ('the NPPF') sets out the Government's planning policies for England and how these are expected to be applied, including guidance for plan-making and decision-making.

2.12 The NPPF confirms that in order to achieve sustainable development, the planning system has three overarching interdependent objectives, including:

- An economic objective – to help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure;
- A social objective – to support strong, vibrant and healthy communities, by ensuring that a sufficient number and range of homes can be provided to meet the needs of present and future generations; and by fostering well-designed, beautiful and safe places, with accessible services and open spaces that reflect current and future needs and support communities' health, social and cultural well-being; and
- An environmental objective – to protect and enhance our natural, built and historic environment; including making effective use of land, improving biodiversity, using natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including moving to a low carbon economy.

2.13 This SPD has been prepared with full regard to the policies set out within the NPPF, as well as the Planning Practice Guidance which supports it.



**This SPD has been prepared with full regard to the policies set out within the NPPF, as well as the Planning Practice Guidance which supports it.**





## Atom Valley

2.14 Atom Valley is a Mayoral Development Zone (MDZ) within the North-East Growth Corridor identified in Places for Everyone, and established in August 2022. This MDZ incorporates the three strategic allocations identified within the North East Growth corridor within PfE along with other key development sites including Kingsway Business Park. Collectively the sites within Atom Valley have the potential to deliver 1.6 million square metres of employment space, 20,000 high-quality jobs and 7,000 new homes.

2.15 Atom Valley is led by a partnership of developers, industrialists, universities, and local government. The vision is to create an extensive and interconnected manufacturing “mega-cluster”, which builds on existing local strengths in advanced manufacturing, materials, and machinery in the local area. Bringing together world-class research, manufacturing, research and development, it aims to shape the future while levelling up communities.

2.16 Beyond providing ample space and opportunity, Atom Valley MDZ stands out as an unparalleled industrial development in the UK and beyond, thanks to the combination of Manchester's leading research facilities and leisure opportunities. Situated at the heart of a vibrant city region, it fosters collaboration among businesses and institutions, with quick access to the motorway network and Manchester city centre just seven miles away.

2.17 As a long-term, large-scale regeneration proposal, this project will play a pivotal role in boosting the competitiveness of the north of Greater Manchester and enhancing the overall prosperity of the city region.

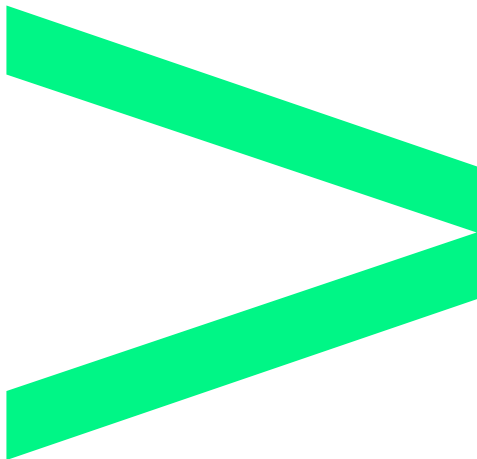
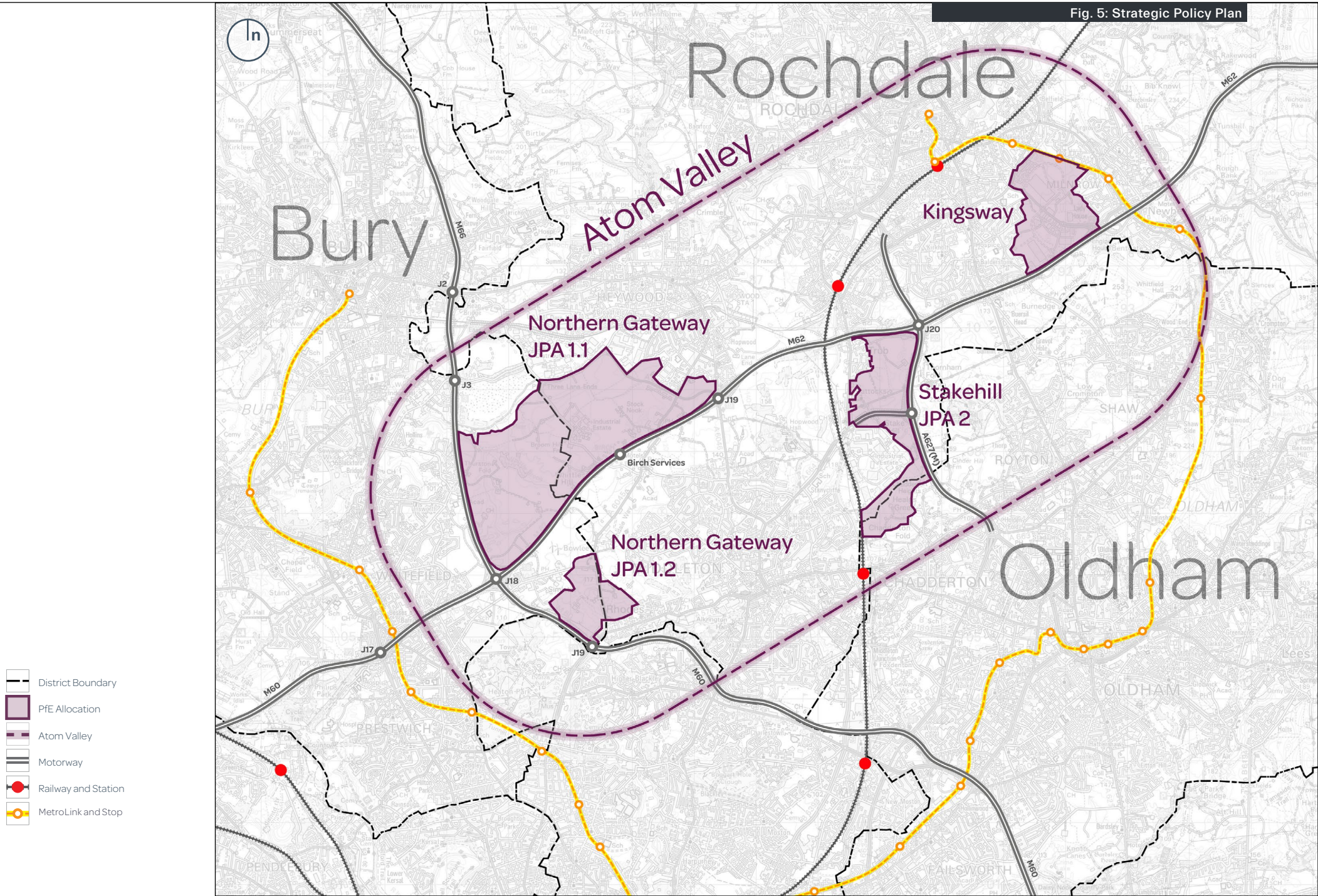




Fig. 5: Strategic Policy Plan





## Alignment with Corporate Strategies

**2.18** This NGDF has been prepared with full regard to the existing corporate strategies of Bury and Rochdale Councils, which are summarised below.

### Bury 2030 Strategy, 'LET'S Do It!'

**2.19** In July 2020, Bury Council and NHS Bury Integrated Care Partnership launched an ambitious community strategy, LET'S Do It!, which sets out a vision that by 2030 the borough of Bury will stand out as a place that is achieving faster economic growth than the national average, with lower than national average levels of deprivation.

**2.20** The Let's Do It Strategy is underpinned by the guiding principles of:

- Local neighbourhoods: empowered residents, families and local communities within the townships at the heart of decision making to make a difference to people's lives.
- An enterprising spirit: Harnessing that spirit to raise aspirations, remove barriers, and maximise opportunities to grow and develop for local people and businesses.
- Delivering together: To deliver a new relationship between public services, communities and businesses which is based on co-design, accountability and shared decision making to focus on wellbeing, prevention, and early intervention.
- A strengths-based approach: a borough in which people are helped to make the best of themselves, by recognising and building on the strengths of all local children, families and communities and taking an evidence-led understanding of risk and impact to ensure the right intervention at the right time.

**2.21** The Bury 2030 Strategy seeks to build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity, which specifically includes land at Northern Gateway.

### Bury's Economic Strategy 2024–2034

**2.22** The Economic Strategy sets out the framework and priorities to guide and steer Bury Council and its partners' collaborative approach to delivering a sustainable, competitive, inclusive and resilient local economy that fulfils its growth potential, while making a significant contribution to Greater Manchester's wider economic growth ambitions over the next decade and beyond.

**2.23** Whilst Bury's economy is integral to supporting Greater Manchester's growth agenda, it is recognised that there is a distinct imbalance even within the Greater Manchester economy with economic output from the north of the conurbation, including Bury, falling behind that of the central and southern areas of Greater Manchester.

**2.24** Through a clear, comprehensive and ambitious strategy, Bury is well placed to play a key role in driving forward sustainable economic growth in the borough and in Greater Manchester. This will be achieved through the realisation of identified key opportunities. For example, the realisation of significant new and higher-skilled employment opportunities through the release of employment land at Atom Valley / Northern Gateway, which presents a 'game changing' opportunity for Bury and for Greater Manchester.

### Rochdale's 'People, Place and Planet Council Plan 2028'

**2.25** Through this strategy Rochdale Council have set out an ambition to work collaboratively to reduce inequalities and deliver sustainable opportunities and services that will benefit our people, place and planet.

**2.26** This is underpinned by various key priorities, which includes achieving a thriving, fairer and sustainable economy by providing space and support for high value business development, innovation and job creation; supporting people in attaining and maintaining good employment; creating more supply, quality and choice in the housing market (social and high value homes); and enhancing transport systems to make it easier for people to access services, jobs and amenities.

## Greater Manchester Transport Strategy 2040

**2.27** The Greater Manchester Transport Strategy 2040 sets out Greater Manchester's long-term ambition for transport, with a vision for the city region to have "world-class connections that support long-term sustainable economic growth and access to opportunity for all".

**2.28** The strategy sets out a strong commitment to provide a transport system which: supports sustainable economic growth and the efficient and effective movement of people and goods; improves the quality of life for all by being integrated, affordable and reliable; protects the environment and supports the target to be net zero carbon by 2038 as well as improving air quality; and capitalises on new technology and innovation.

**2.29** It sets a vision (referred to as the 'right mix vision') that 50% of all journeys in Greater Manchester are made by walking, cycling and public transport by 2040. This will mean one million more sustainable journeys every day enabling the delivery of a healthier, greener and more productive city region.

## Northern Gateway Transport Framework

**2.30** A Transport Framework has been developed specifically for Northern Gateway and considers multi-modal accessibility to the site including the potential and patronage for sustainable travel options, linked to the vehicular access strategy. A transport programme which includes sustainable transport schemes, associated phasing and possible funding sources for priority transport interventions will be included within the Framework. The framework is a live document which will feed into the Northern Gateway Infrastructure Phasing and Delivery Strategy and planning applications will need to align with this strategy.

## Bury Local Transport Strategy

**2.31** The Bury Local Transport Strategy was approved by the Council in October 2023 and sets out a plan for transport investment in Bury for the next 20 years and beyond, covering all modes of travel in the borough. The strategy aligns with the 'Let's Do It' vision for Bury and the Greater Manchester Transport Strategy 2040 and will be used to help secure investment for transport in the Borough.

**2.32** The strategy seeks to link residents to new opportunities and to deliver on the Council's inclusive growth ambitions by connecting people to jobs, education, leisure and tourism. Ensuring that Bury residents can access Northern Gateway and the potential new jobs is a key priority within the strategy. Key strategic transport interventions that are expected to be required to support delivery of the Northern Gateway site are also identified, including junction improvements for the M66 (junctions 2 and 3) to facilitate vehicular, public transport and active travel movement.

## Rochdale Borough Transport Strategy

**2.33** The Rochdale Borough Transport Strategy (2014 refresh) provides a framework to guide the development of transport improvements across the Borough for the period up to 2026. It seeks to ensure that, by 2026, the Borough has an affordable, sustainable, reliable, accessible and integrated transport network that offers travel choice for all, serves its communities, tackles air quality and climate change, enhances social inclusion, public health and supports economic growth and regeneration of the local area.

## Planning History

**2.34** In Bury, planning permission has not been granted for any significant uses within the site.

**2.35** In April 2024, National Highways submitted an application seeking a Development Consent Order (DCO) for the M60 / M62 / M66 Simister Island Interchange, on the south western corner of the JPA1.1 site. The improvement scheme seeks to reduce peak congestion and facilitate future economic growth across Greater Manchester, including supporting the delivery of proposed development sites close to the M60 and M66. The extent of the physical works are identified on the Development Framework plan. The application is currently under examination. If the DCO is made, it would allow National Highways to acquire compulsory interests in and rights over land, including parcels of land within the JPA1.1 site (temporarily and permanently).

**2.36** In Rochdale, hybrid planning permission was originally granted in March 2020<sup>2</sup> for land at South Heywood, within the northeastern part of the site:

- The ‘detailed’ component of the hybrid planning permission granted full planning permission for a new link road between Junction 19 of the M62 and Pilsworth Road, which has now been completed and is open for use (known as Queen Elizabeth II Way).
- The ‘outline’ component of the hybrid planning permission granted outline planning permission (with all matters reserved except for access) for a major mixed-use development comprising up to 135,460 sqm of employment floorspace (Use Classes B2/B8), 1,000 new homes, a new local centre and primary school and associated landscaping, sports pitches, drainage, ecological enhancements, cycleway and footpath linkages and ancillary infrastructure. Following the grant of reserved matters approval, the early phases of development are now under construction.

**2.37** A further outline planning permission was granted in January 2023<sup>3</sup> for an additional 40,000 sqm of employment floorspace (Use Class B8) on land within the South Heywood Masterplan Area. As a result, the total amount of employment floorspace approved at South Heywood is 175,460 sqm.

**2.38** It is anticipated that the South Heywood Masterplan Area will continue to be brought forward through the aforementioned permissions, demonstrating compliance with the outline parameters, comprehensive planning condition requirements and approved Design Code, whilst providing infrastructure in line with the established S106 Legal Agreement.

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2. RMBC ref. 16/01399/HYBR

3. RMBC ref. 22/01569/OUT







# Vision and Strategic Objectives

Atom Valley and Northern Gateway represents a once in a generation opportunity for Greater Manchester



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## 03. Vision and Strategic Objectives

Atom Valley and Northern Gateway represents a once in a generation opportunity for Greater Manchester in the form of a nationally significant development capable of supporting the rebalancing of the national economy and providing transformative change to local communities and the wider region with regard to investment, jobs and skills.

### Atom Valley Ambition

- **Goal:** Covering sites across Bury, Oldham and Rochdale, this vast employment opportunity will play a pivotal role in making Greater Manchester one of the best places in the world to grow up, get on and grow old.
- **Scope:** Spanning 1.6 million sqm of employment land and creating 7,000 low carbon homes, Atom Valley prioritises sustainable development and the creation of a lasting legacy of high-quality employment sites that will benefit local communities for years to come.
- **Impact:** Generating 20,000 high-quality jobs, fostering a skilled workforce to drive economic growth and innovation in advanced sustainable materials and providing flexible employment opportunity for a wide variety of other sectors and industries. Beyond sites, it transforms the fabric of the region, enhancing liveability and prosperity.
- **Objective:** Establish a nationally significant industry hub for levelling up, prioritising sustainability, innovation, and collaboration between manufacturing and logistics, with an Advanced Sustainable Materials Translational Research facility and Advanced Manufacturing Park.
- **Approach:** To develop a strong private-public partnership that will provide the necessary infrastructure to accommodate a diverse range of employment uses.
- **Outcome:** Atom Valley pioneers large-scale employment investment, fostering innovation and prosperity across the north of the Greater Manchester conurbation and beyond. As a nationally significant industry hub, Atom Valley accelerates national prosperity and champions the levelling up agenda, securing a legacy of holistic progress in economy, society, and environment.



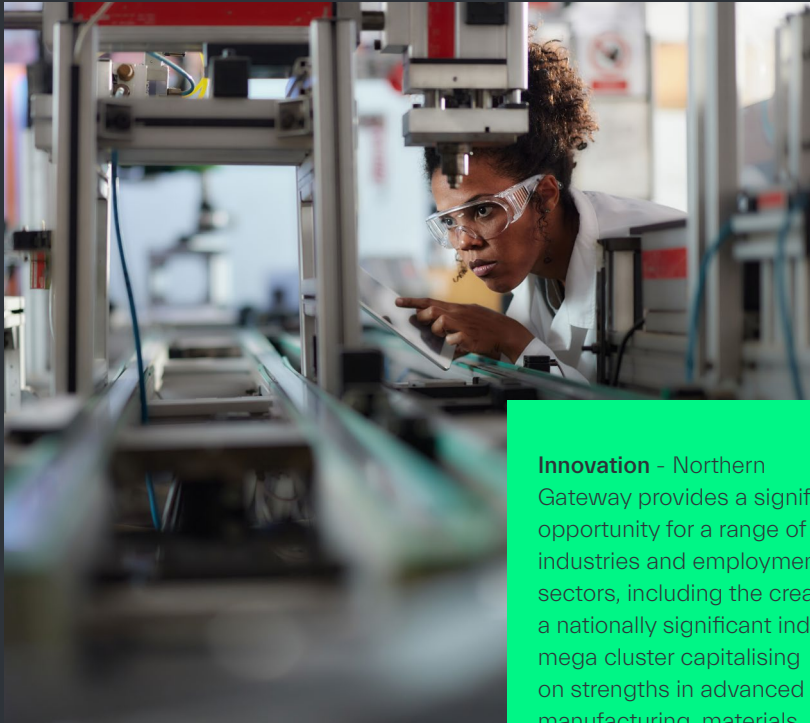
## Northern Gateway Strategic Development Vision

3.1 Northern Gateway will be a world-class business community that will help to drive inclusive growth and economic prosperity for Atom Valley and Greater Manchester by becoming one of the largest and most accessible employment opportunities in the region, delivering:

- Significant investment into a range of industry sectors at both scale and pace, supporting the region's economy and resilience.
- A dynamic and thriving innovation hub for advanced materials and manufacturing, building on Greater Manchester's world leading strengths in this area.
- A vibrant and dynamic employment ecosystem where businesses and communities thrive, supported by sustainable transport connectivity and enhanced green and blue infrastructure networks which help adapt to future climate change.
- Flexibility and responsiveness to market demands and opportunities, and
- A truly integrated approach to partnership working with industry, universities, research partners and institutions, that will develop and harness the skills for the local workforce, establishing a legacy of excellence in training and skills, empowering existing communities as well as attracting and retaining top industry talent.



# Northern Gateway: Pillars of development

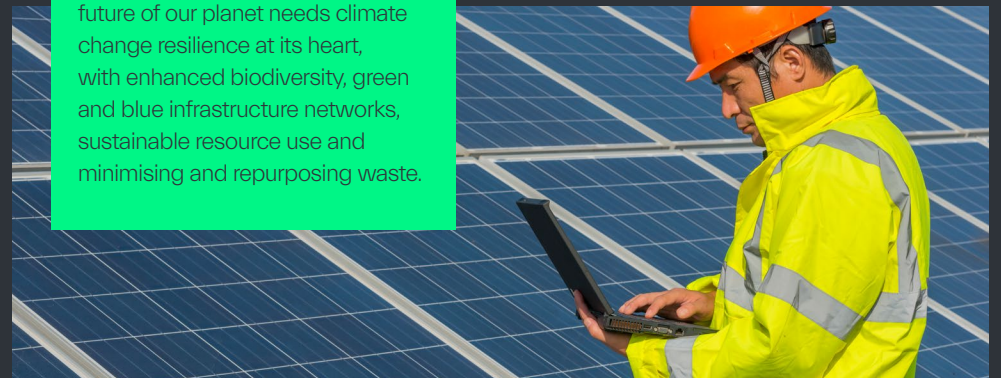


**Innovation** - Northern Gateway provides a significant opportunity for a range of industries and employment sectors, including the creation of a nationally significant industrial mega cluster capitalising on strengths in advanced manufacturing, materials, and machinery, enhancing national prosperity and delivering levelling up.



**Inclusive Communities** - Creating a new and inclusive community with 2,750 homes (across the JPA1.1 and JPA1.2 sites), including low carbon and affordable homes. Empowering local communities through skills development initiatives, fostering economic inclusivity and social mobility, helping to address national and regional inequalities.

**Sustainability** - Northern Gateway will contribute to Greater Manchester's commitment to be net zero carbon by 2038. The future of our planet needs climate change resilience at its heart, with enhanced biodiversity, green and blue infrastructure networks, sustainable resource use and minimising and repurposing waste.







#### **Business and industry -**

Northern Gateway will attract UK and international firms investing in new distribution and manufacturing premises, and SMEs scaling-up into modern premises, supply chains providing resilience to drive economic growth and spinouts and start-ups ready to move from the lab to the factory to benefit from international standard R&D and skills facilities.



**Collaboration** - Partnerships with Government, developers, industry, colleges and institutions will attract, retain and develop top talent. We will create a long-term legacy, driving skills and nurturing talent to deliver transformational change to the conurbation.



**Connected Places** - A modern, affordable and reliable public transport service, with active travel provision and enhancement, will provide a sustainable, connected network of travel routes, linking existing residential areas with new business premises and facilities, providing access to jobs and health and wellbeing benefits.

## Northern Gateway: 12 key principles

- 1 **Catalyst for Transformation:** Northern Gateway provides a significant opportunity for economic growth across various employment sectors, boosting the productivity and prosperity of the north of the city region.
- 2 **Strategic Location:** Strategically positioned as a central driver of regional progress, leveraging its connectivity to become a focal point of economic activity within Greater Manchester. Its strategic location has the ability to attract national and inward investment from a multitude of sectors including advanced manufacturing, distribution and logistics.
- 3 **Advanced Manufacturing:** Spearheading investment and innovation in advanced materials and manufacturing, building on Greater Manchester's world leading strengths in this area and creating a leading hub for innovation and sustainable industry growth.
- 4 **Scalability and Adaptability:** Over 700 hectares providing unparalleled scalability and adaptability to meet the diverse needs of a range of industries and evolving market dynamics, ensuring flexibility and responsiveness to market demand.
- 5 **Strategic Partnerships:** Collaboration between private investors, developers, funders, universities and agencies will cultivate innovation at Northern Gateway, driving inclusive growth and enhanced productivity across a multitude of sectors. Collaboration with Universities will be integral to enabling commercialisation of innovation at scale.
- 6 **Focal Point & Hub:** Fostering collaboration and innovation among businesses, creating a vibrant ecosystem that attracts and nurtures talent, enhancing Greater Manchester's competitiveness, focused on a sustainable materials catapult.
- 7 **Integrated Sustainability Approach:** Sustainability will be embedded across all aspects integrating with the natural environment from infrastructure to operations, demonstrating a commitment to environmental responsibility, high quality green and blue infrastructure and BREEAM / Well / LEED-certified initiatives.
- 8 **Job Creation and Skills Development:** Focus on creating job opportunities and empowering local communities through skills development initiatives, fostering inclusive growth and building a skills legacy for businesses and communities.
- 9 **Integrated Transport Solutions:** Integrated public transport solutions will be implemented at Northern Gateway to improve accessibility and connectivity, offering efficient and sustainable transportation options for residents and workers and connecting to nearby communities.
- 10 **Promotion of Active Travel:** Northern Gateway will deliver pedestrian-friendly pathways, cycling lanes, bridleways and green and blue spaces to promote active mobility, link surrounding communities and enhance connectivity, providing health and wellbeing benefits for local residents, future residents and workers.
- 11 **Flexible Approach:** Northern Gateway will embrace a flexible approach, allowing for adaptable adjustments to meet the evolving needs and opportunities of Greater Manchester's dynamic landscape and to reflect the sheer scale of the site.
- 12 **Maximising opportunities for existing communities:** Significant opportunity to deliver new jobs and economic growth in one of the regions most deprived areas through strategic partnerships, investment in skills and training and sustainable transport and active travel connectivity.





# Local and Strategic Outcomes

The proposals will bring a range of benefits that will embrace the three pillars of sustainable development



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## 04. Local and Strategic Outcomes

The proposals will bring a range of benefits that will embrace the three pillars of sustainable development, as follows:

- **Economic:** Creating a world class employment park that attracts the very best logistics and manufacturing industries.
- **Social:** Delivering fully inclusive growth that delivers a permanent positive social legacy.
- **Environmental:** Ensuring the development will not have a detrimental impact on the environment by maximising opportunities to provide net gains for biodiversity, creating a comprehensive green infrastructure network and ensuring that the objective of net zero carbon for both the development and the Greater Manchester area is integral to the design from the outset.



**Northern Gateway will evidently create significant economic benefits, however, both councils and Greater Manchester have documented the expectation that the breadth of benefits realised through new development will extend beyond the generation of jobs and associated investment in skills and training.**

## Economic Benefits

**4.1** The preceding sections capture the scale and breadth of economic benefits that will be expected to be realised through the delivery of Atom Valley. The development of the site is expected to make a substantial contribution to the realisation of this vision and the aspiration to deliver new good quality jobs across the Atom Valley area.

**4.2** It is recognised that the scale of new employment opportunities created on the site will build upon and benefit from close proximity to existing regionally renowned employment sites at Heywood Distribution Park and Pilsworth. The development of the site will also complement other key sites in the north of the sub-region such as Logistics North in Bolton. Cumulatively these employment areas will represent a significant concentration of employment opportunities for existing and future residents of Greater Manchester.

**4.3** The strategies reviewed in Chapter 2 affirm the value of these jobs in ensuring a more positive future for the residents of Greater Manchester, and particularly those within the northern parts of the conurbation that are currently disengaged from the economy or are not realising the full extent of their potential economic contribution. This acknowledges the sustained challenges associated with inter-generational deprivation within communities near the proposed development.

**4.4** These strategies also affirm the value placed on the creation of higher-value and higher-skilled jobs in addressing long-standing economic productivity inequalities within the conurbation and at a larger spatial scale. There is an expectation that the development will serve to attract business investment that will create the jobs required to increase the retention of highly skilled graduates and attract skilled labour to Greater Manchester.

**4.5** The scale and make-up of the development floorspace to be accommodated will need to respond to market opportunities and occupier requirements and the type and number of jobs created will ultimately be dependent on the nature of the occupiers. However, it is expected, in the context of the above, that the development will provide the opportunity to create a breadth of jobs, including those in higher value sectors and those identified as Frontier or Growth Sectors through the strategies summarised in Chapter 2.

**4.6** Proposals for development will be expected to provide a quantified estimate of the scale of employment opportunities that are anticipated to be realised through both the construction phase and (where relevant) the operational phase of the proposed development. Proposals should set out how the generation of employment opportunities will be expected to have a wider economic and social benefit for local communities.

## GROSS DIRECT GVA PER ANNUM

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£1,490m

## ANNUAL BUSINESS RATE INCOME

---

£36.9m

## ANNUAL COUNCIL TAX RECEIPTS

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£2.7m

4.7 Through implementation of PfE Policy JP-J1 (Supporting Long-Term Economic Growth), the councils will support long-term job growth by seeking agreement with employers and developers to enter into local labour and training agreements through planning obligations and other mechanisms, where appropriate.

4.8 It is recognised, as described in Chapter 9 of this SPD, that the development will be progressed in phases over an extended period. The assessment of economic benefits and mechanisms for capturing these locally will need to demonstrate sufficient flexibility to respond to changing economic circumstances over time and recognise the opportunity to maintain a process of engagement with local stakeholders, including skills and education providers.

4.9 It is expected that consideration will be given as to how local stakeholders will continue to be engaged with through the development process.

## Social Benefits

4.10 The proposed development at Northern Gateway will evidently create significant economic benefits, however, both councils and Greater Manchester have documented in respective (existing and emerging) Social Value Strategies or Policies the expectation that the breadth of benefits realised through new development will extend beyond the generation of jobs and associated investment in skills and training. This recognises that surrounding communities exhibit significant levels of high deprivation; the drivers of which are complex and multi-faceted.

4.11 Proposals will be expected to consider how the proposed development, including the construction phase, will contribute to the aspiration to foster inclusive growth by providing opportunities for disadvantaged and priority groups and supporting the empowerment of local communities.

4.12 New housing provided within the site will have an important role to play in addressing existing housing needs, which include for a range of types and tenures of housing. The provision of high-quality housing has significant social benefits which recognise the value associated with providing a stable home to families and individuals as well as financial benefits associated with more energy efficient housing.

4.13 The provision of new social infrastructure, including community facilities and recreational and leisure facilities, will provide enhanced provision for the new residential areas but also surrounding existing communities to benefit from. These facilities will need to take into consideration existing facilities in proximity and identified gaps in provision.

4.14 The proposed development will provide an important opportunity to enhance existing active travel networks and create new facilities and spaces to promote healthy lifestyles and contribute towards elevated wellbeing through the creation of positive working and leisure environments.

4.15 It will also provide an opportunity through the capturing of commitments to volunteering and support existing local community groups and organisations to build positive relationships between businesses and proximate local communities.

4.16 It is expected that proposals will consider in a proportionate way how the above opportunities will be supported through individual developments and how proposals have taken account of local issues identified by stakeholders and communities.



**The provision of new social infrastructure, including community facilities and recreational and leisure facilities, will provide enhanced provision for the new residential areas but also surrounding existing communities.**



## Environmental Benefits

**4.17** The delivery of a net zero and highly sustainable employment park will support green growth within the region and help attract innovative world-class industry.

**4.18** Development of the site will support the environmental pillar of sustainability through application of the following PfE policies:

- Policy JP-S1: Sustainable Development
- Policy JP-S2: Carbon and Energy
- Policy JP-S3: Heat and Energy Networks
- Policy JP-S4: Flood Risk and the Water Environment
- Policy JP-S6: Clean Air
- Policy JP-S6: Resource Efficiency
- Policy JP-G1: Landscape Character
- Policy JP-G2: Green Infrastructure Network
- Policy JP-G3: River Valleys and Waterways
- Policy JP-G4: Lowlands Wetlands and Mosslands
- Policy JP-G7: Trees and Woodland, and
- Policy JP-G8: A Net Enhancement of Biodiversity and Geodiversity.

## Mitigating Climate Change – Net Zero Carbon and Circular Economy

**4.19** The site will support Greater Manchester's ambition to become a carbon neutral city-region by 2038 by seeking to achieve net zero carbon emission buildings in construction and operation, in line with the requirements of PfE Policies JP-S1 and JP-S2.

**4.20** Operational energy use and carbon will be reduced as far as possible through:

- High levels of fabric efficiency
- All-electric building services strategy, utilising low carbon energy generation such as heat pumps (at network or individual building level)
- Maximising renewable energy generation on site through solar photovoltaic panels and a potential for building mounted micro wind-turbines, and
- Smart energy systems including metering, and the potential for energy storage.

**4.21** Construction carbon emissions will be quantified, and minimised in accordance with the following:

- Lean design e.g. avoiding over-engineering and over-ordering
- Use of natural materials
- Lower carbon options such as steel manufactured using the 'electric arc furnace' process rather than blast furnace
- Use of local suppliers and labour where possible, and
- Reducing energy and water use on site and exploring alternatives to diesel use.

**4.22** Carbon emissions will also be reduced by implementing circular economy measures, which will complement the above measures and are expected to include:

- Material efficiency
- Use of recycled materials and materials with a high recycled content
- Use of materials which can be repurposed or recycled at end of life
- Resource (energy and water) efficiency measures during construction and operation, and
- Reduction of waste generated during construction and operation, and diversion of residual waste from landfill.

**4.23** The measures listed above will deliver a development which limits contribution to climate change and supports the transition to a net zero carbon future.

## Adapting to Climate Change – Resilience

**4.24** Greater Manchester will experience changes to climate in the future, including:

- Increased annual temperatures
- Increased winter rainfall, and
- Decreased summer rainfall.

**4.25** All development within the site will be expected to incorporate measures which demonstrate adaptability and resilience to climate change including:

- Reducing overheating risk through passive measures prior to implementing active cooling
- Reducing flood risk through Sustainable Drainage Systems (SuDS), without significantly increasing embodied carbon emissions
- Incorporating green and blue infrastructure to mitigate overheating, flood risk, and contribute to biodiversity
- Maximising opportunities for climate resilience through the design and construction of new buildings e.g. green roofs
- Minimising water use during operation through reducing water demand and incorporating rainwater or greywater harvesting, and
- Providing an appropriate landscaping strategy which supports local biodiversity suits current and future climate scenarios.

**4.26** The measures listed above will deliver a development which is adaptable and resilient to climate change and limits environmental impacts to biodiversity, building occupants, and the local community.

## Wider Environmental Benefits

**4.27** The buildings delivered at the site will demonstrate environmental benefits through application of accreditation schemes such as BREEAM.

**4.28** BREEAM considers the full range of environmental impacts to demonstrate how sustainability has been applied holistically, including:

- Management
- Health and Wellbeing
- Energy
- Transport
- Water
- Materials
- Waste
- Land Use and Ecology, and
- Pollution.

**4.29** The environmental benefits of the site including net zero carbon, climate resilience and adaptation, and wider measures such as building accreditation will be delivered in combination with the economic and social benefits detailed above.





# Understanding of the Site and Surroundings

This chapter represents an essential step between the Strategic Policy Context, Vision and Strategic Objectives and the Local and Strategic Outcomes



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**This chapter represents an essential step between the Strategic Policy Context, Vision and Strategic Objectives and the Local and Strategic Outcomes set out in Chapters 1 - 4, and the requirements, proposals, parameters and guidance set out in Chapters 7 - 10.**

## Strategic Location

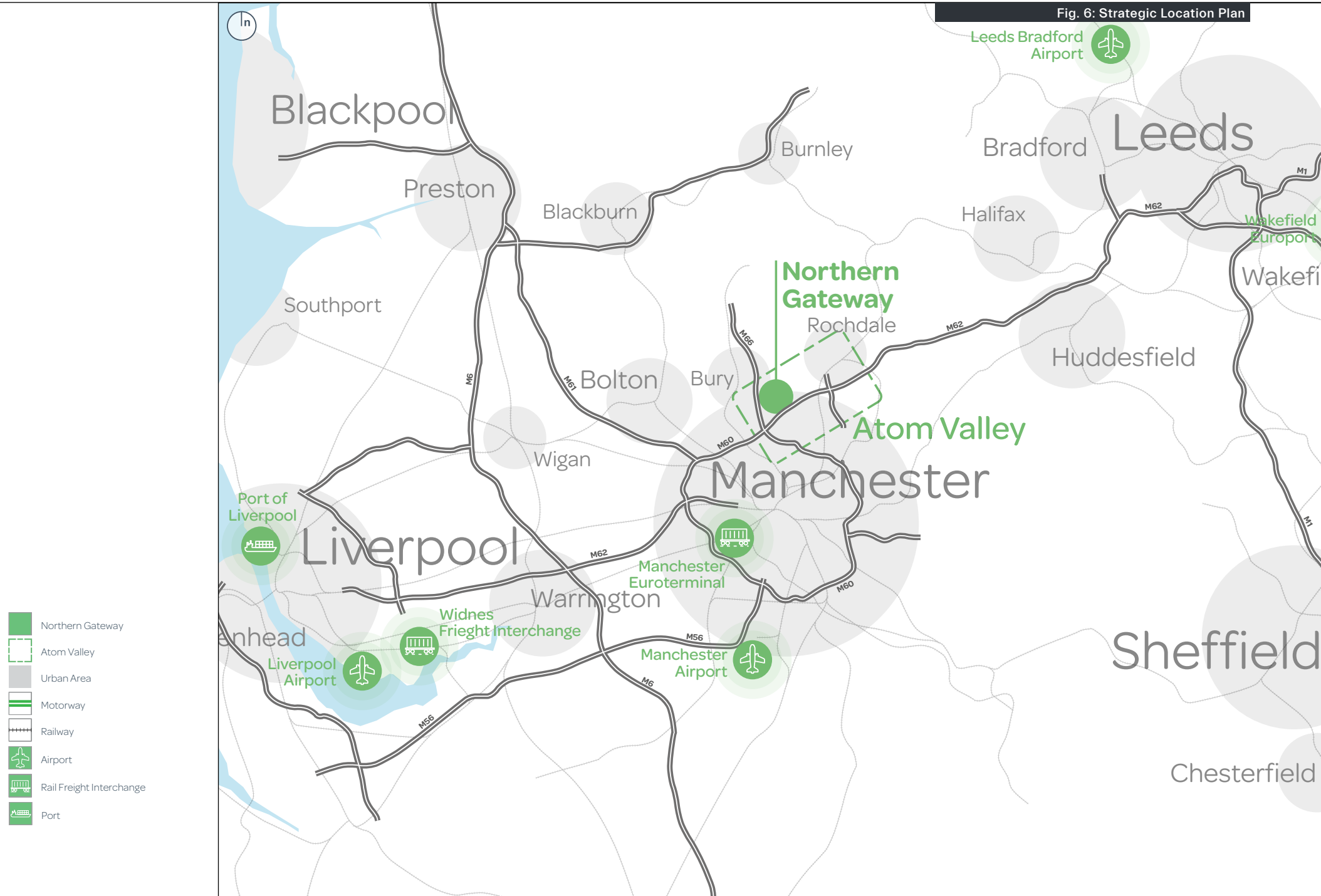
5.1 Northern Gateway sits at a crucial juncture, encompassing the convergence of the M60, M62, and M66 motorways. This strategic and highly accessible location provides the site direct access to the UK motorway network. Within an hour's drive there are three major cities with international airports, two freight ports, and the West Coast Main Line, all supported by the strategic road network. The M60 facilitates connections to the rest of the Greater Manchester conurbation, as well as facilitating access into Manchester City Centre itself, just 7 miles away. The site benefits from being in close proximity to existing regionally renowned employment sites at Heywood Distribution Park and Pilsworth Industrial Estate.

5.2 Northern Gateway, alongside Kingsway Business Park and Stakehill, are within the Atom Valley, Manchester's Mayoral Development Zone (MDZ). Atom Valley is an ambitious employment initiative set to become an extensive and well-connected innovation cluster generating 20,000 high-quality jobs. Bringing together world-class research, manufacturing, and materials, it aims to shape the future while levelling up communities.

5.3 Frequent and wide-reaching rail services are available from Rochdale Station with direct connections to Manchester, Leeds, Chester and Blackburn. For rail freight, the site is 35 minutes from Manchester Euroterminal, 50 minutes from Wakefield Europort and two hours fifteen minutes from the Hams Hall (West Midlands) railhead.



Fig. 6: Strategic Location Plan



5.4 International and domestic flights and air cargo services are available from both Manchester Airport and Liverpool John Lennon Airport. As the UK's second-largest airport, Manchester currently handles around 100,000 tonnes of freight annually, connecting the North of England to c.200 destinations around the world. Manchester Airport lies approximately 23 miles away from the site, reachable within a 25-minute drive. In addition, Leeds Bradford Airport and Liverpool John Lennon Airport are both situated within an hour's drive from the site, offering further travel options.

5.5 Northern Gateway is around an hour's drive from the port of Liverpool and approximately two hours from Humberside's ports. Liverpool handles over 30 million tonnes of freight annually, although its new super port has recently opened to accommodate the world's largest container vessels, and Humberside has the main UK ports for a number of European, Scandinavian and Baltic destinations.

5.6 The Manchester Metrolink, renowned as the UK's largest light rail network, boasts several stations conveniently located within a short distance of the Northern Gateway, including Bury, Whitefield, and Rochdale Town Centre.

5.7 As part of the Greater Manchester Transport Strategy 2040, there is potential to introduce a Metro / tram-train service linking Bury and Rochdale, contingent upon securing funding and receiving approval based on the business case. This would utilise a section of the East Lancashire Railway, a leisure route between Heywood and Rawtenstall, which lies north of Heywood Distribution Park.

5.8 Rochdale and Bury stand as the nearest main town centres to Northern Gateway. Both boast lively and diverse retail options, complemented by an array of cultural attractions such as museums, art galleries and entertainment venues. Additionally, a multitude of dining and drinking establishments, and an indoor and outdoor market at Bury, add to the vibrant atmosphere. These towns also offer ample traditional community amenities, including schools, medical facilities, and green spaces. Heywood, which lies to the north of the site, provides a similar range of services on a more local scale.





## Site Conditions

**5.9** Analysis of the site has been undertaken to identify key constraints and opportunities associated with the site and its surrounding context. These constraints and opportunities form a strong evidence base to the plans and guidance included within this NGDF.

**5.10** A diagrammatic summary of the opportunities to be maximised, and constraints to be considered arising from the technical assessments is set out in Figure 14 at the end of this chapter.

## Transport and Highways

**5.11** The site is defined by a number of roads and highways along a majority of its boundaries as follows:

- M62 Motorway to the South
- M66 Motorway to the West
- Pilsworth Road to the North, and
- Queen Elizabeth II Way to the East.

**5.12** In addition, the junction of the M62 and M66 with the M60 Motorway ('Simister Island') abuts the site to the South West and provides connections across Greater Manchester.

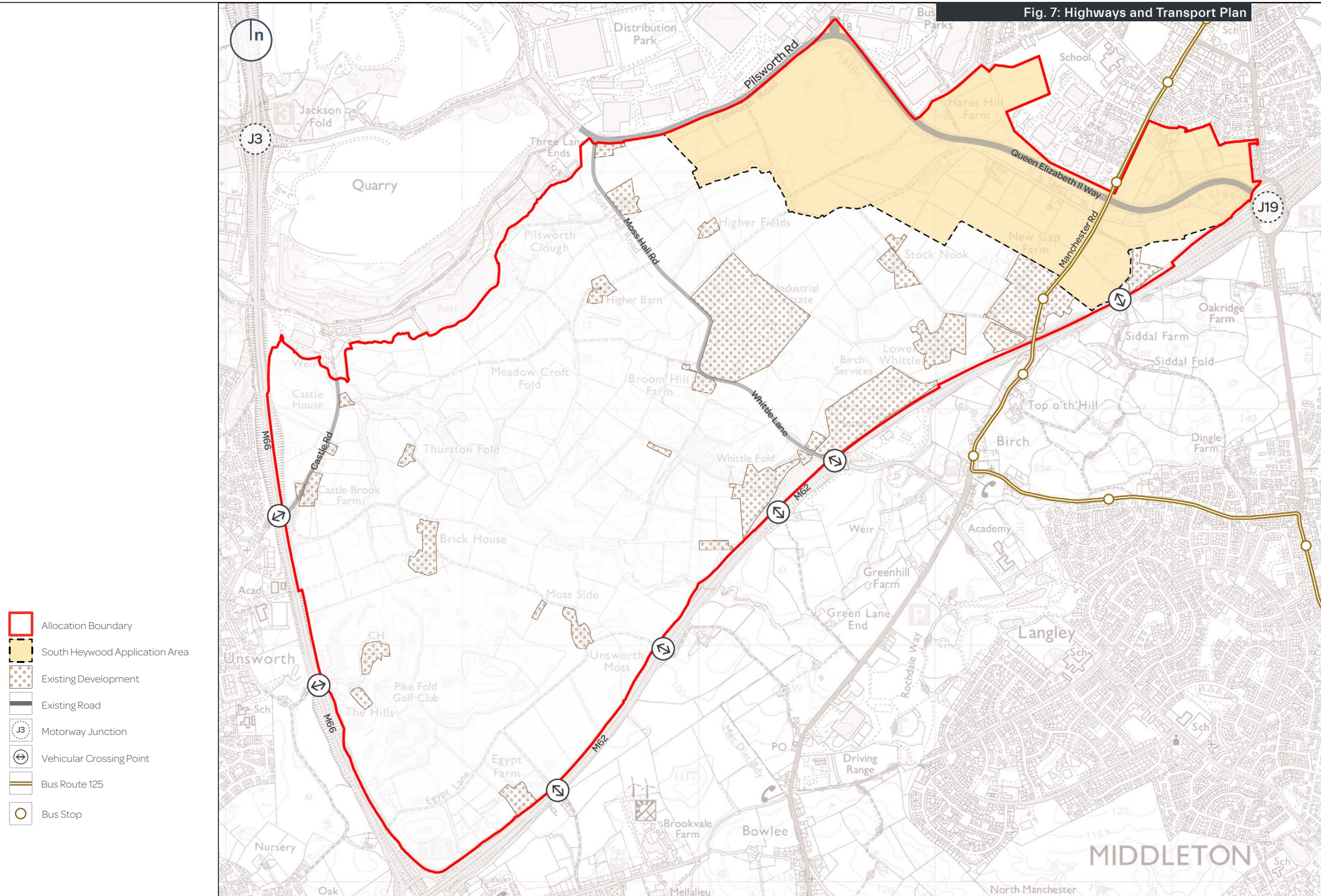
**5.13** Access into the site is gained off Pilsworth Road along the site's northern boundary which provides direct access to Junction 3 of the M66 Motorway to those traveling north/south, and off Queen Elizabeth II Way along the site's eastern boundary which provides direct access to Junction 19 of the M62 Motorway to those travelling east/west.

**5.14** Access into the site is also possible via Whittle Lane/ Moss Hall Road which runs north/south through the middle of the site, as well as Castle Road which crosses part of the site in the west and Manchester Road which crosses the eastern part of the site. Vehicle access into the site across the M62 Motorway is provided by Whittle Lane, Manchester Road and Queen Elizabeth II Way to the south, whilst Castle Road is the only vehicle access into the site over the M66 Motorway to the west.

**5.15** There are a number of other existing vehicle crossing points which cross both the M62 and M66 Motorways into the site, but these are not designed for general traffic and provide 'access only' to a number of individual farms, dwellings and Pike Fold Golf Course. These crossing points are not deemed suitable for providing vehicular access for future proposals but will be retained/enhanced/modified for the purpose of providing active travel routes across the motorways as the precise layout of the site and specific access points become more developed.

**5.16** The 125 Bus Service which serves the easternmost part of the site and connects Heywood and Middleton is the only bus route which currently serves the site offering 1x hourly services in each direction. Other bus services are accessible in Heywood to the east and Unsworth to the west.

Fig. 7: Highways and Transport Plan



**5.17** The approved South Heywood Masterplan currently being implemented in the easternmost part of the site includes provision for a number of new bus stops to accommodate new/extended bus routes in order to serve the development. This will likely significantly increase accessibility to the eastern part of the site in the next few years.

**5.18** A number of proposals are currently being developed that have the potential to increase capacity on surrounding road and transport infrastructure. They include the following interventions:

- M66 Link Road, also known as the Western Access, which spans several roads and M66 Junction 3
- M66 Junction 2/3 improvements
- M66 Junction 2 / A58 localised junction improvements
- M62 Junction 19 / A6046 Heywood junction improvements
- Castle Road junction improvements, and
- Highway improvements on the local network.

**5.19** In addition to the above, proposals to extend Junction 19 of the M62 are also in the early stages of development. Whilst the scale and form of such proposals are yet to be agreed, they have the potential to provide a new point of access into the southern part of the site providing direct access on to the M62.

**5.20** Future proposals for the site will need to be developed in accordance with emerging highway infrastructure works and will therefore need to be drafted in consultation with key stakeholders including National Highways, Transport for Greater Manchester (TfGM) and the local highways authorities. In accordance with the vision for Northern Gateway and wider strategic goals, proposals should also prioritise the promotion of more sustainable forms of transport and a modal shift away from private road transport through the utilisation of high-quality public transport and active travel connections.

## Public Rights of Way

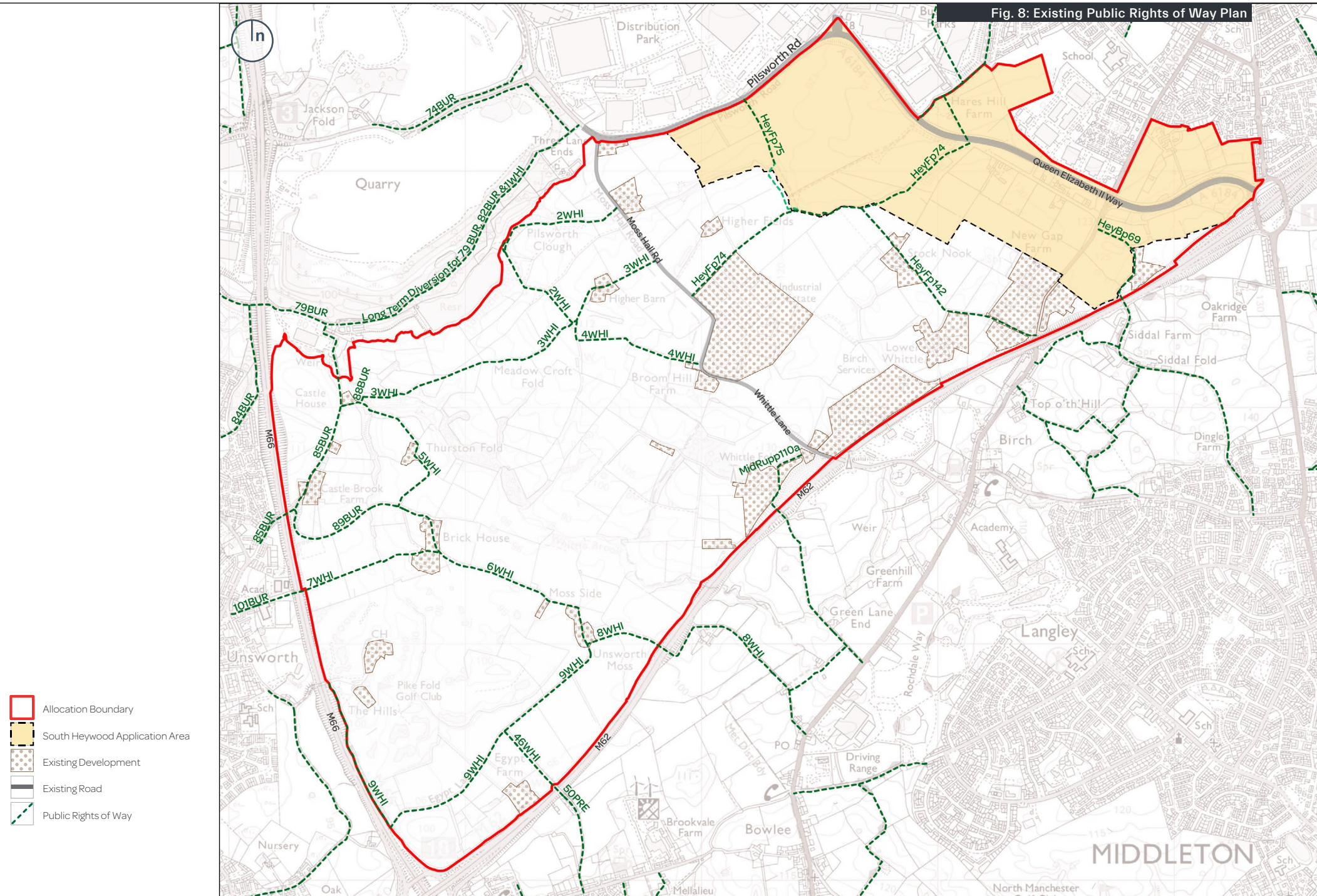
**5.21** There are several Public Rights of Way (PRoW) crossing the site with many extending beyond the boundaries, forming good connectivity with the wider countryside, however the site is currently inadequately accessible for the local community as many of these routes are not readily identifiable on the ground, difficult to maintain, and provide reduced access for those with mobility issues, and are therefore infrequently used.

**5.22** In the western extent of the site, the majority of the PRoW run along macadam surfaced farm access tracks. In the central extent of the site there is a good PRoW network, however in places these footpaths are unclear. Footpath 3WHI is a key link running across the centre of the site, connecting Castle Road to Moss Hall Road.

**5.23** There are four PRoW which cross the M62 via single lane bridges, from east to west these are: MidFp114a, MidFp110 on Whittle Fold, 8WHI on Simon Lane and 46WHI on Egypt Lane linking the settlement of Simister to the site. Footpath 85BUR passes over the M66 on Castle Road whereas 7WHI passes underneath through Unsworth Academy grounds.

**5.24** The aspiration at Northern Gateway is to create a permeable development, accessible to all, through the implementation of a well-maintained and high-quality footpath/cycleway/bridleway network with retained and new links to the surrounding area. Where feasible, the existing PRoW should be retained in-situ, however it is expected that some existing PRoW may need to be diverted to facilitate development and take the opportunity to create a coherent and attractive route network.





## Landscape and Visual Character

### *Landscape and Recreation Designations*

**5.25** There are no World Heritage Sites, National Parks or National Landscapes (previously known as Areas of Outstanding Natural Beauty) located within close proximity of the site. Part of the site is currently located within a Special Landscape Area, as designated in the Bury Unitary Development Plan.

**5.26** The site is located within National Character Area (NCA) 54 Manchester Pennine Fringe, with NCA 55 Manchester Conurbation located to the south.

**5.27** The Greater Manchester Landscape Character and Sensitivity Report (2018) identifies the site as being located within three Landscape Character Types (LCTs); predominantly 'Mosslands and Lowland Farmland' and 'Urban Fringe Farmland', with the area around Brightley Brook to the north of the site falling within 'Incised Urban Fringe Valleys'.

**5.28** There are also three Landscape Character Areas (LCAs) within the site, namely LCA 25: River Roch, LCA 26: Prettywood, Pilsworth and Unsworth Moss, and LCA 27: Simister, Slattocks and Healds Green. The Greater Manchester Landscape Character and Sensitivity Report identifies LCA 25, associated with the River Roch, as having 'moderate to high' sensitivity to commercial development and 'moderate' sensitivity to residential development. It should be noted that only a small part of the site at the northern boundary around Brightley Brook falls within this LCA. LCA 26 and 27, which cover the majority of the site, are assessed as having 'moderate' sensitivity to both commercial and residential development.

**5.29** The site includes existing sports provision namely Unsworth Academy sports pitches and Pike Fold Golf Club. These facilities should be retained and enhanced or, where necessary, provision for replacement facilities should be made that are equivalent or better in terms of quantity and quality and in a suitable location. Any development adjacent to the playing fields may also be required to provide suitable mitigation to ensure that the recreational value of these areas is not impacted by the new development.

### *Visual Baseline Summary*

**5.30** The surrounding views are an important aspect of the visual amenity of the site, with long distance views available to the north being a key characteristic. Local receptors within the site and outside the boundaries consist mainly of farms and associated residences, as well as users of the public footpath network through the site. Within the north eastern part of the site industrial units dominate views, giving an overall more industrial feel to this part of the site. The Pilsworth landfill site also forms a prominent feature on the skyline to the north.

**5.31** The M62 forms a dominant feature in the view to the east/south-east along with its associated infrastructure and lighting. The vertical elements of pylons, telegraph poles and lighting columns are a continuing theme throughout the site.

**5.32** Development at Northern Gateway should be incorporated successfully into the local landscape setting. This should be achieved through the provision of accessible and high quality green and blue infrastructure; and where possible, the retention, replacement and enhancement of the brooks and their u-shaped valleys, mature trees, hedgerows and woodland blocks.

**5.33** Long distance views are available out of the site and across the site from the M62. Retention of such views should be considered where feasible to maintain the connection of the site to the wider landscape.







## Topography

**5.34** The topography of the site is situated within a transitional zone between elevated, open moorlands and densely populated urban areas on lower ground.

**5.35** The southern extent of the site comprises mainly flat fields at a similar level to the adjacent M62 motorway allowing direct views between the site and motorway. A small area of localised mounding is present in the south-western extent of the site adjacent to the M62/M66/M60 junction. Pike Fold Golf Course is an undulating site with a parkland character.

**5.36** The western extent of the site, between the M66 and Whittle Brook (with the exception of Pike Fold Golf Course mentioned above) comprises gently undulating fields dissected by Castle Brook heading south from Whittle Brook. Whittle Brook follows a wooded u-shaped valley as it dissects the site.

**5.37** The mid-section of the site between Whittle Brook and Whittle Lane/Moss Hall Road comprises more undulating landform than the west, which rises above the adjacent motorway. The area has a more heavily vegetated feel and the change in landform enhances this characteristic. U-shaped valleys of the brooks add character to the site and create distinguishable features. Although the brook valleys are dry in some areas, the landform clearly shows their former presence.

**5.38** The eastern extent of the site lies to the east of Whittle Lane/Moss Hall Road. Farm properties at Stock Nook and Lower Whittle accessed from Dr Fold Lane are located within undulating pastureland with outgrown hedge field boundaries creating a similar character to the midsection of the site. There are also linear groups of woodland. The landform begins to fall away to the north before rising again in this area, allowing long views in a northerly direction. The far eastern extent of the site comprises mainly flat fields which are currently under development as part of the approved South Heywood Masterplan.

**5.39** The design of the site will be seeking to regrade topography and create development plateaus. A cut and fill balance should ensure to minimise the impact on the existing setting. The plateaus should provide scale and flexibility to unit sizes to attract a variety of occupiers whilst the distinguishable u-shaped valleys of the brooks should be retained to maintain these landscape features.

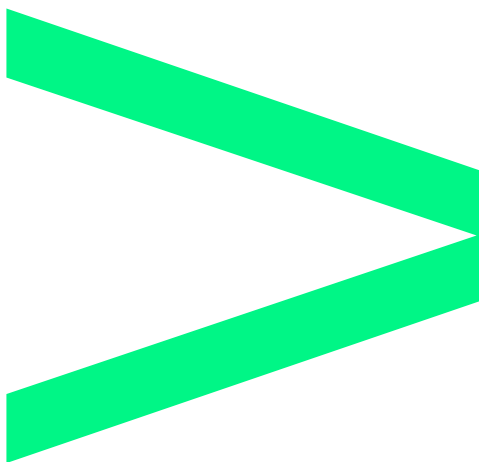
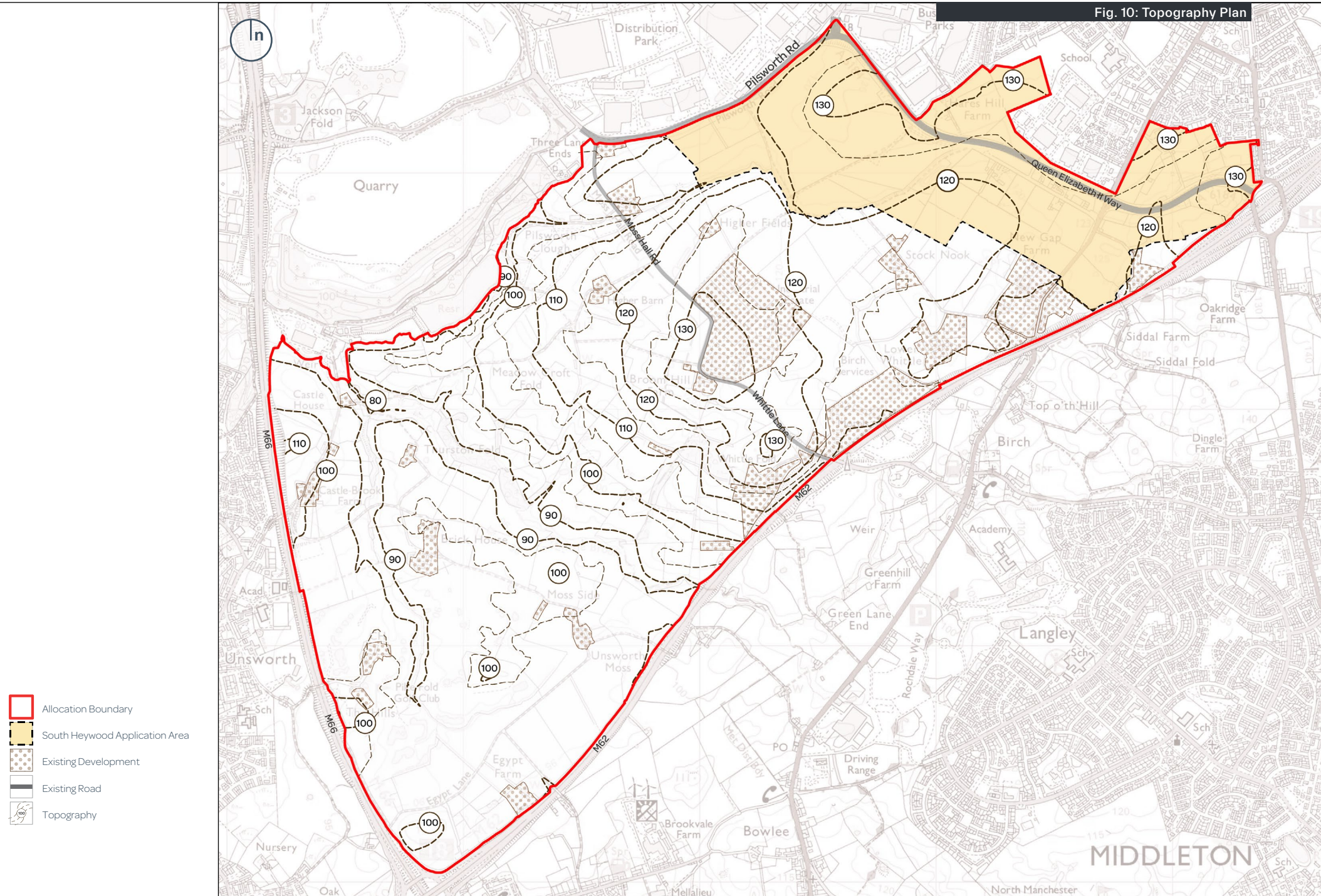


Fig. 10: Topography Plan







### Arboriculture

**5.40** The site's existing tree stock comprises a mix of individual trees, collective groups of trees and pocket areas of woodland of largely good to moderate quality spread throughout the site. The majority of tree cover is situated within established field boundary hedgerows, with the addition of sporadic, incidental pockets of tree cover within the field parcels.

**5.41** The most notable tree stock comprises areas of mature woodland that is situated on the banks and within influence of Whittle Brook as well as several mature individual specimen trees. The woodland provides a notable contribution to the site's landscape, whilst providing a natural habitat corridor through the western part of the site. Additionally, there are a number of spurs that veer off from the woodland to the east, all of which are of moderate to high quality.

**5.42** Other notable tree covered areas are those that align the eastbound carriageway of the M62, and those that align the southbound carriage way of the M66. These trees provide a good buffer from the adjacent carriageways, in addition to contributing to the local landscape. Trees situated within the bounds of Pike Fold Golf Course broadly comprise linear strips of structural tree planting and sporadic crescents of trees aligning the fairways and greens respectively.









**5.43** There is a vast network of hedgerows that span across the entire site, situated almost exclusively at the field parcel boundaries, or aligning the internal private access roads. These are of moderate quality in terms of their arboricultural merits, species diversity and contribution to habitat connectivity across the site.

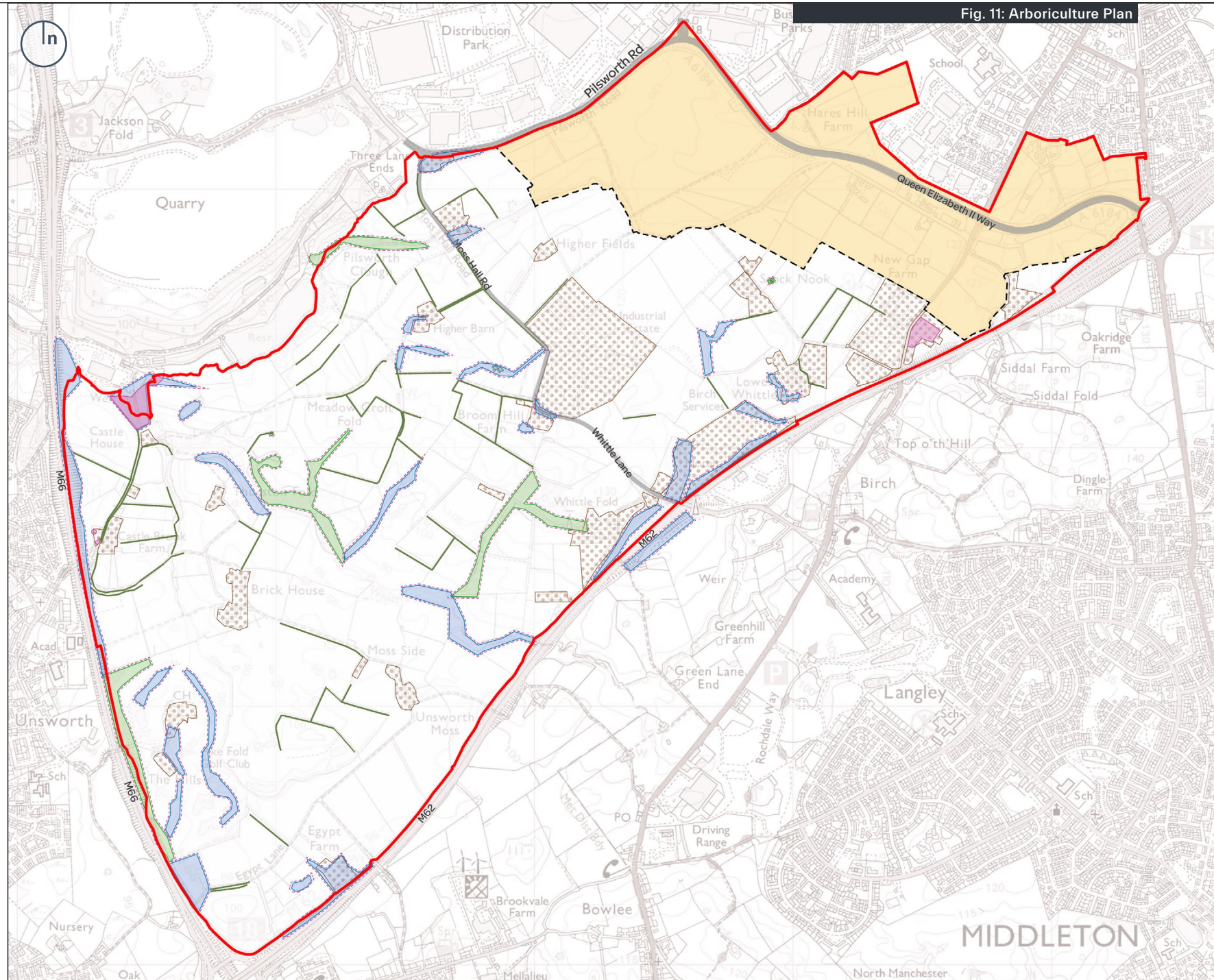
**5.44** A landscape-led approach should be applied to ensure the retention and enhancement of natural assets, where feasible, and their integration into the development. The existing mature tree cover and trees with notable amenity value should be used to inform the green infrastructure network within and across the site. Where natural assets can be retained, suitable development buffers should be applied.

**5.45** Any tree and hedgerow losses should be mitigated on-site through the creation of new tree planting, woodland enhancement and hedgerow connections. Given the widespread nature of the site, there is an opportunity to increase and strengthen tree canopy and hedgerow cover across the site with new native planting set within a connected green infrastructure network.



Fig. 11: Arboriculture Plan

-  Allocation Boundary
-  South Heywood Application Area
-  Existing Development
-  Existing Road
-  High Retention Value Trees and Hedgerow (CAT A) and associated Development Offset
-  Moderate Retention Value Trees and Hedgerow (CAT B) and associated Development Offset
-  Tree Preservation Order (TPO)
-  Indicative Location of Established Hedgerow (CATB - Moderate Retention Value)







## Ecology

**5.46** There are no European or Nationally designated sites within the site. The only European or Nationally designated site within 2km of the site is the Rochdale Canal Special Area of Conservation (SAC) and Site of Special Scientific Interest (SSSI) which is located c. 1.8km to the east of the allocation boundary. There are two Local Nature Reserves (LNR) within a 2km radius of the site – Hopwood Woodlands LNR approximately 1.7km to the east and Hollins Vale LNR approximately 100m to the west on the other side of the M66. There are ten Sites of Biological Importance (SBI) within a 2km radius of the site. Pilsworth SBI lies to the immediate north of the site.

**5.47** The site predominantly comprises improved grassland and occasional fields/areas of poor semi-improved grassland, both of which are of low ecological value. There are occasional fields/areas of semi-improved neutral grassland and areas of marshy grassland within parts of the site which had poor drainage, both of which are considered of good ecological value.

**5.48** The most ecologically valuable habitats (in terms of intrinsic value and wider value to wildlife) are situated along the Whittle Brook and tributaries corridors supporting a mixture of woodland, scrub and riparian grassland habitat. Four broadleaved semi-natural woodlands, identified as Habitats of Principal Importance (HPI), are located along the site boundaries; two are mature woodlands associated with Pilsworth SBI located along the northern site boundary. The site also contains a number of open water ponds and a number of field ditches which provide good ecological value.

**5.49** The scrub/grassland across the Whittle Brook and tributaries corridors have been demonstrated to support birds of conservation concern including Barn Owl and Grasshopper Warbler. The agricultural grassland has also been shown to support ground-nesting birds of conservation concern such as Lapwing and Skylark. A number of the ponds have been shown to support or have potential to support Great Crested Newts. The site also is known to support a range of other protected and notable wildlife including farmland and wintering birds, roosting bats, badgers, brown hare and riparian mammals.

**5.50** The ecological strategy for the site will target benefits for the above species and apply best practice principles of the mitigation hierarchy outlined in the GMCA's guidance for biodiversity net gain and also BS:42020: avoidance, minimisation, restoration, offsetting. The hierarchy should be applied at the earliest design stages.

**5.51** It will be necessary for future development at Northern Gateway to compensate for habitat losses where avoidance and mitigation measures are not possible. Compensation should be provided on-site and only where it is robustly evidenced that this is not possible, off-site measures could be considered to ensure priority habitats expected for loss are compensated for on a like-for-like basis and development overall achieves a net gain in biodiversity in line with national and local policy requirements. Where any off-site compensation is demonstrated as being necessary, this should be secured in the local area wherever feasible.

## Flood Risk and Drainage

**5.52** Ecological improvements should include the retention and enhancement of the Whittle Brook and tributaries corridors and associated riparian habitats to support Water Framework Directive objectives as well as areas of woodland in the site and alongside the motorway edges. Additional green infrastructure corridors should be created throughout the site which enhance habitat connectivity on site and to neighbouring habitats off-site. Future development should have regard to the Greater Manchester Local Nature Recovery Strategy and the potential opportunities to improve and join up wildlife sites which contribute to Greater Manchester's Nature Network. Development should be directed to lower value habitat areas whilst retaining and enhancing those areas of most value wherever possible such as the ponds, watercourses and woodland. There must be an overall improvement in habitat value which should be supported with a management plan for the site to promote wildlife.

**5.53** There are four watercourses within the site that are classified as Main Rivers. Whittle Brook flows from south east to north west across the site. Castle Brook flows south to north and converges with Whittle Brook and is renamed to Hollins Brook. Brightley Brook flows from east to west through the northern area of the site and also converges with Hollins Brook, a tributary of the River Roch.

**5.54** Environment Agency mapping identifies that the majority of the site is located within Flood Zone 1 (i.e. land assessed as having a 0.1% or lower annual probability of river flooding). There are areas along the banks of both Whittle Brook and Brightley Brook that have been identified as areas of Flood Zone 2 (i.e. land with a between 0.1% and 1% annual probability of river flooding) and Flood Zone 3 (i.e. land with a 1% or higher annual probability of river flooding).

**5.55** Development on, or within 8 metres for the bank top, of a designated main river flow will require a flood risk activity permit or exemption granted from by the Environment Agency. Watercourses should be retained and enhanced including the provision of an ecological corridor either side of these watercourses informed through comprehensive ecological assessments having regard to the Water Framework Directive and current flood risk processes up to the modelled 1 in 1000 plus climate change extent(s).

**5.56** Surface water flooding within the site is considered low risk. There is an intricate network of overland flows and ponding throughout the site (at various levels of risk) which drain overland into existing watercourses. However, this is a large site with the potential to create significant volumes of runoff and therefore it will be necessary for these to be managed through Sustainable Urban Drainage Systems to ensure the flows are released at a low rate to prevent peaks in runoff. The Development Framework should maintain

the natural overland flow routes and design out the areas of ponding which would present a flood risk to structures in that area.

**5.57** A surface water drainage strategy for the whole site should seek to reduce flood risk where possible through the widescale use of sustainable drainage features in accordance with the CIRIA SuDS Manual and the Greater Manchester Sustainable Drainage Design Guide. To ensure that flood risk is not increased at the site or elsewhere as a result of the development, surface water runoff from the development should be restricted to the existing greenfield runoff rate. Flood risk from the development should be mitigated through these features and water quality should be improved by following the recommendation for pollution prevention for large scale commercial/industrial schemes.

**5.58** No public surface water sewers have been identified within the site (beyond the South Heywood Masterplan Area). Surface water run-off will be, where shown to be appropriate, discharged into the ground through multiple infiltration structures or to the watercourses within the site at the limiting discharge rates.

**5.59** No public foul sewers were identified within the development boundary (beyond the South Heywood Masterplan Area), therefore foul water will be pumped to a new or existing point of discharge specified by United Utilities, outside of the boundary of the site.

**5.60** In line with PfE Policy JP-D1, engagement with United Utilities to develop an acceptable surface water and foul water drainage strategy for any future proposal will be required and is likely to include upgrades of the existing public sewer network and nearby Bury Wastewater Treatment Works.





## Built Heritage

**5.61** There are no World Heritage Sites, Scheduled Monuments, Registered Battlefields or Protected Wrecks within the site or in close proximity. There are six Listed Buildings, one Registered Park and Garden and one Conservation Area immediately surrounding the site. The Listed Buildings are all Grade II, and date from the postmedieval to the modern period, of which the below two are within the site:

- Brick Farmhouse (1067266) is a Grade II Listed Building. It is a presumed 17th century brick 2-storey building, with front rendering and 20th century renovation. It has a dated tablet inscribed 'Adam/Gafell/Octob 21/1981' and is thought to be the oldest brick-built farmhouse in the area. This lies on Griffe Lane in the west of the site.
- Lower Whittle Farmhouse (1068502) is a Grade II Listed Building and dates from the 17th century and is a timber-framed structure with substantial 18th century rebuilding of parts and 19th century renovations. This lies off Doctor Fold Lane in the east of the site.

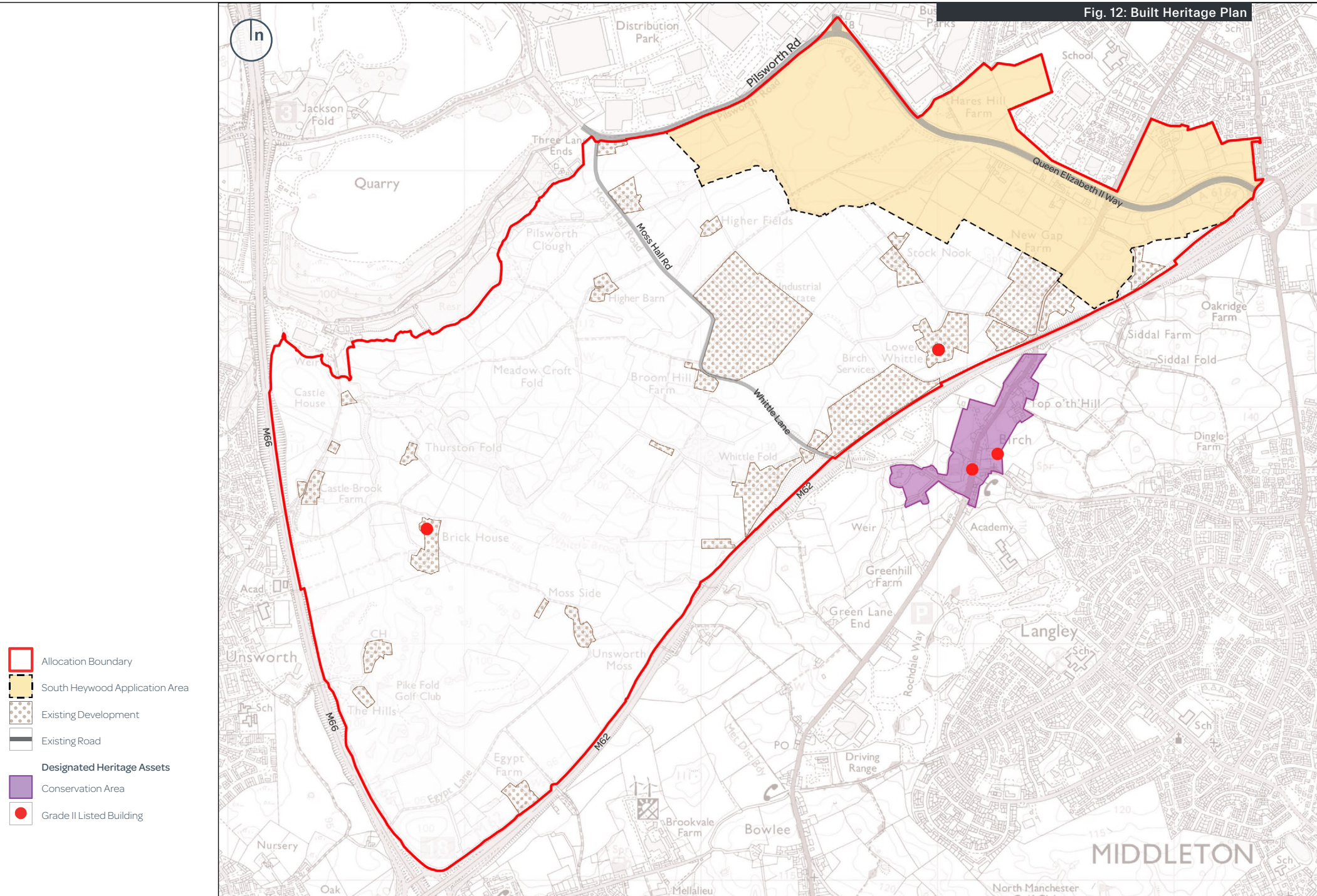
**5.62** The Registered Park and Garden, Heaton Park (1000854), is Grade II registered and dates from the post-medieval period. The Conservation Area of Birch Village (DGM4496) is comprised of a characteristic combination of early 19th century industrial architecture and late 19th century high-quality Arts and Crafts style residences. Both designations are outside the site but within the 1km study area.

**5.63** In addition, there are 13 Locally Listed Buildings (non-designated heritage assets) within the site and also 13 Greater Manchester Historic Environment Record (HER) entries with further entries sited just outside of the boundary to the site. Towards the centre of the site, one of the most significant Locally Listed Buildings within the site is Meadow Croft Fold, which includes a derelict two-storey Georgian farmhouse, with two brick-built threshing barns to the north. The farmhouse itself is currently in a poor condition after extensive fire damage.

**5.64** Listed Buildings, and their setting, within the site should be incorporated into the Development Framework to preserve the heritage of the area. The potential impact of any development on designated assets within the 1km study area and surrounding landscape should be considered.



Fig. 12: Built Heritage Plan



## Archaeology

**5.65** There are no archaeological constraints that would preclude development of the site however, there are several areas of potential archaeological interest within the site.

- Meadow Croft Fold - Archaeological assessment of Meadow Croft Fold to date, including a programme of trial trenching and shovel Test Pitting, has confirmed this area as the site of a Medieval iron smelting works and small-scale settlement. Further mitigation in the form of a strip, map and record programme of works is recommended before development.
- Whittle Brook - Whittle Brook is a suggested iron smelting site based on the results of archaeological investigations in 1984. Whilst these investigations are not conclusive in proving that a bloomery (a type of furnace for smelting iron) existed here, it is a possibility.
- Unsworth Moss and Siddal Moor - Preserved organic palaeo environmental remains may be present at Unsworth Moss and Siddal Moor, due to the areas of peat and previously recorded deposits within the locality.
- Castle Brook - The earthwork remains at Castle Brook Farm may be indicative of a prehistoric settlement/camp, with the feature situated on a well-drained spur above Castle Brook.

**5.66** In advance of development within the site further geophysical surveys, archaeological evaluation and palaeo environmental sampling would be undertaken as required and archaeological mitigation strategies may subsequently be required in agreement with GMAAS. Future development at the site will subsequently be informed by the results of this programme of archaeological works.

## Utilities

**5.67** The ability to provide key services for the site is a fundamental aspect of delivery and has been considered from the outset. The following summary sets out how the development can be served by sufficient electricity, gas, water and telecommunications, as well as identifying any existing services across the site that have informed the Development Framework. As this work progresses, the provision of utility infrastructure will be set out in the Infrastructure Phasing and Delivery Strategy.

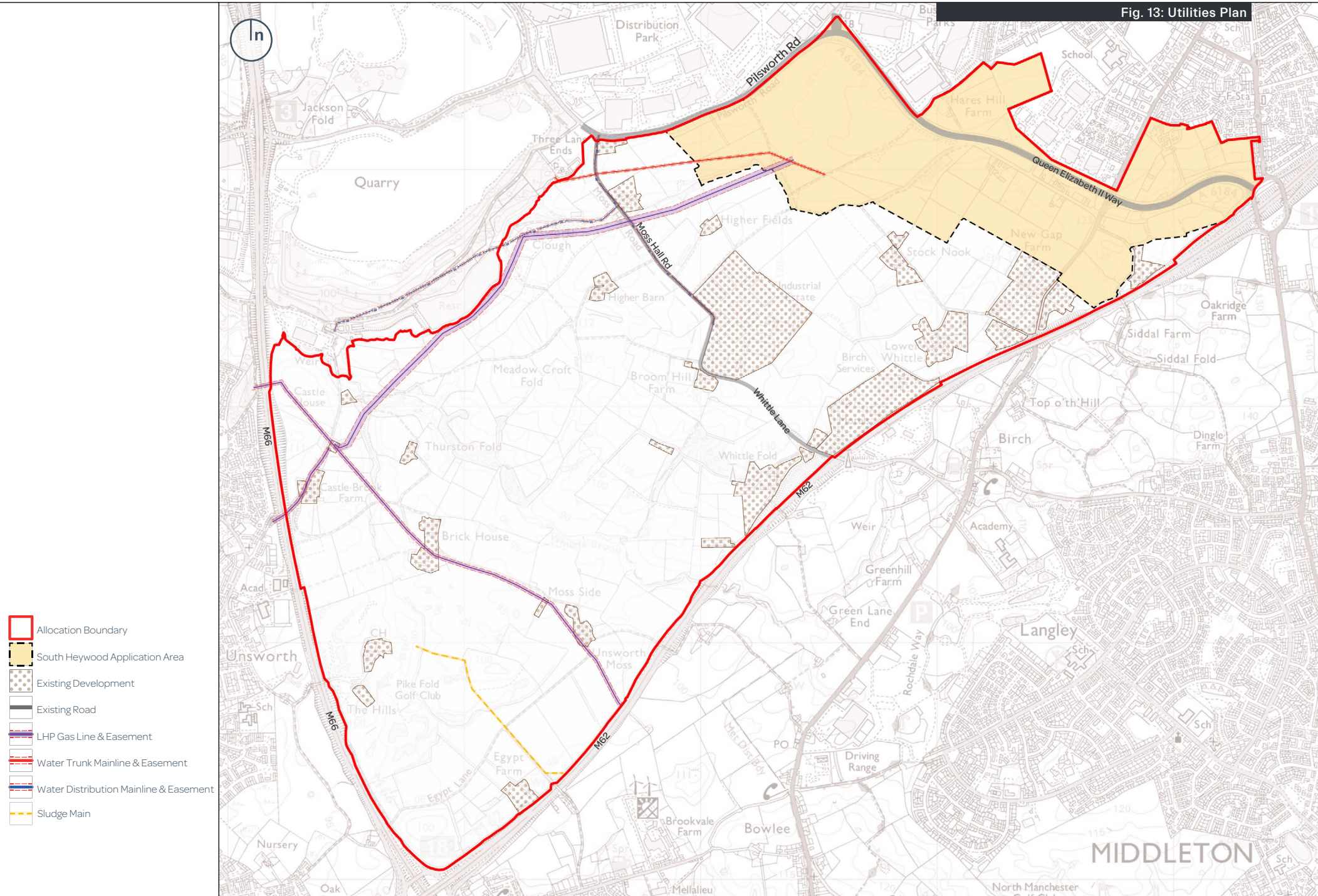
- Electricity – Electricity North West anticipate that a new primary substation will be required to serve the proposed development, and a Point of Connection has been identified. From the new primary substation a further network of 11kV substations will be distributed throughout the site.
- Gas – There are localised high pressure gas mains within the site. The Warthfold – Middleton pipeline runs through the northern part of the site and is subject to an easement of 6 metres along with a building proximity distance restriction of 8 metres extending either side of the pipeline from the centre point. The Whitefield – Castleton pipeline runs from north west to south east across the site south of Whittle Brook to the M62. The pipeline is subject to a 12 metre easement along with a building proximity restriction of 15.5 metres extending either side of the pipeline. The Phillips Park intermediate pressure pipeline lies underneath Castle Road and is subject to an easement of 9 metres and a building proximity restriction of 6 metre extending either side of the pipeline from the centre point. Cadent Gas have confirmed that the current mains have sufficient capacity to support the load required for the development without any reinforcement works. These are key constraints which have

been accommodated within the Development Framework as Green Infrastructure and active travel corridors and should be incorporated in all future development at Northern Gateway.

- Water – There are two water mains running through the northern part of the site. There may be the potential to divert these to accommodate development and re-modelling works should be undertaken to provide a water supply within United Utilities standards of service which is suitable for the end use of the development.
- Telecommunications – Telecommunications are already being implemented through the South Heywood Development to provide BT Openreach and Virgin Media communication network coverage. The operators will extend their coverage to provide high speed data and fibre networks throughout the development. Further engagement with telecommunications groups will inform any potential new and upgraded infrastructure for mobile telecommunications to provide the new businesses, population and workforce, with reliable and high-speed connectivity.
- Sludge main - There is sludge main running from Pike Fold Golf Course south east where it crosses the M62 Motorway. The main is under the control of United Utilities who will need to be notified ahead of any works being undertake in close proximity to the main. An easement of 3.5m either side of the main is required. There may be the potential to divert these to accommodate development but this is subject to agreement with United Utilities.



Fig. 13: Utilities Plan







## Noise

**5.68** Parts of the site are dominated by road traffic noise sources from the M62 to the south of the site and M66 to the west. Noise from farming, commercial and industrial uses are also present in parts, within isolated areas within and adjacent to the site. Generally, there are a limited number of noise sensitive receptors within and around the site however these include private residential premises.

**5.69** The incorporation of key design measures will mitigate against any adverse noise impacts at Northern Gateway. Appropriate mitigation measures for the development may include adequate separation distances between 'noisy' employment uses and residential properties; or orientating service yards/access routes away from residential properties where possible, and the use of localised screening in the form of bunds or fences as well as restriction of deliveries / servicing times within areas closest to residents. Such mitigation should be considered at the outset of a design concept, and where proposed development is close to sensitive noise receptors (such as housing) then applicants should clearly demonstrate how noise mitigation has been carefully considered to protect the amenity of existing and future housing.

**5.70** In addition to the relationship between the residential uses and the proposed employment uses, measures should be incorporated into the design of the new residential areas to provide mitigation to existing sources of noise, which is predominantly dominated by road traffic. Mitigation should include incorporation of a minimum stand-off distance of 50 metres from the nearest motorway carriageway, orientation of buildings towards noise sources (with gardens positioned to the rear to screen private external amenity areas) and the use of boundary treatments, such as bunds or barriers, between residential areas and motorway boundaries to provide additional screening from sources of transportation noise.

## Air Quality

**5.71** Motorway emissions associated with the M66 and M62 are considered the greatest source of emissions surrounding the site. Greater Manchester have declared an Air Quality Management Area for Nitrogen Dioxide (NO<sub>2</sub>) attributed to the motorway network. Future development traffic has the potential to increase pollutant levels in the area and affect levels within the Air Quality Management Area. However, with the implementation of best practice air mitigation measures, in the form of sustainable transport options, it is not expected that there will be adverse effects in terms of air quality as a result of the development.

**5.72** Any stand-off from the motorways required due to the noise constraints for residential elements of the scheme is likely to be sufficient as a form of mitigation for Air Quality for any future occupiers of dwellings. It is not considered that there are likely to be any air quality risks associated with the commercial aspect of the development. However, where possible, mitigation should be considered.

**5.73** Opportunities exist to improve the environment with respect to air quality through the design of the scheme. This should include positioning sources of emissions (e.g. spine roads) away from sensitive receptors where feasible, the provision of green and blue infrastructure networks to provide health benefits to workers and residents and provision of Electric Vehicle “fast charge” points across the development. Additional measures should encourage sustainable means of transport, including cycling and walking, through the delivery of improved public transport infrastructure and layouts with good accessibility as well as a comprehensive Travel Plan to educate occupiers and encourage use of these measures.

## Geo-Environmental

**5.74** No geotechnical or geo-environmental constraints have been identified which would prevent future development of the site.

**5.75** The site predominantly comprises agricultural land which is not expected to be significantly contaminated. The following potential geo-environmental contamination risks have been identified along with potential mitigation below:

- Pilsworth Landfill - Ground gas protection measures may be required for any new build residential properties.
- Historic area of bleach works in the north east of the site - Remediation of contamination and where possible reuse of the end material.
- Backfilled quarries/pits - Assessment of material and remediation and, where possible, re-use of material.
- Ground gas migration from mineworkings and generation from the peat - Grouting of mineworkings. Ground gas protection measures may be required in new build residential properties.
- Lignite in peat in the south west - Placement of lignite at depth if it is present near surface or removal off site.
- Potential for contamination of groundwater - Consideration of contamination if groundwater present in contaminated parts of the site.

**5.76** Intrusive ground investigation, assessment and detailed remediation to mitigate these risks should be undertaken before future development comes forward. Whereby land contamination is known or suspected, investigation and assessment work should be carried out by a suitably competent person(s) and follow the government’s published Land Contamination Risk Management (LCRM) guidance. Prior to the undertaking of any intrusive works, it should be ensured that any necessary permissions are obtained or exemptions secured. Whereby uncertainty exists over permission requirements, preliminary engagement with the relevant competent authorities will be necessary.

**5.77** The British Geological Survey (BGS) geological map shows an area of peat deposits within the far south west corner of the site, as shown on Figure 14. However, two site investigations of this area have been undertaken which confirm that there are no peat deposits in the vast majority of this area. Peat deposits are limited to the east of the southbound slip road of the M66 and west of Egypt Lane. Within this area, peat is present in isolated locations to depths which are no greater than 0.75m below ground level (bgl) and in most cases far less. The limited presence of peat within the site will not provide a constraint in terms of geo-technical design and is unlikely to be significant in terms of loss of a soil resource.

## Geo-Technical Summary

**5.78** Geotechnical aspects to consider at the site include mining, compressible peat deposits, backfilled quarries and pits and groundwater presence. Intrusive investigation should be undertaken before future development comes forward to assess these aspects further and identify standard mitigation measures as required. The ground should be characterised for cut and fill works and for foundation design.



## Site Constraints and Opportunities






















**5.79** As set out in previous sections, there are no known technical constraints which would preclude the proposed development of the site. The following constraints and opportunities have been important considerations in the design of the Development Framework Plan and should be considered in future development at the site.

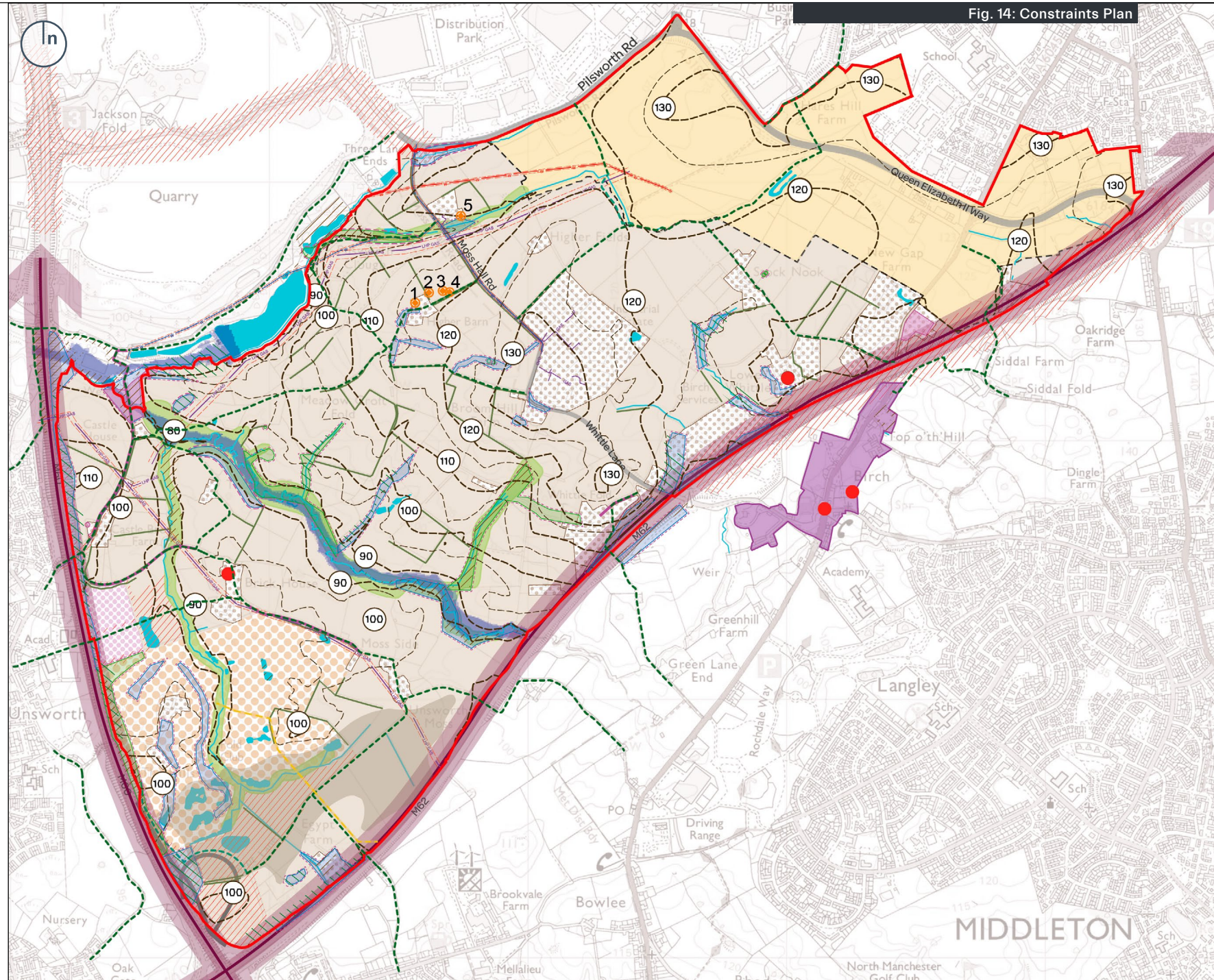
### Constraints

- Current capacity of the existing two main access points from the M62 J19 and Pilsworth Road.
- Mineshafts, probable shallow coal mine workings, within the northern area of the site.
- Watercourses across the site - Whittle Brook, Castle Brook and Brightley Brook with areas of Flood Zone 2 and 3 along their banks. Buffer zones should be applied.
- Mature trees, hedgerows and woodland blocks are present throughout the site. Where possible these should be retained and enhanced. Where retention is not possible, adequate compensation for any losses should be provided.
- Existing recreation facilities within the site, including Unsworth Academy sports pitches and Pike Fold Golf Course.
- Ecological habitats including ponds, trees, grasslands and buildings with bat potential. Mitigation required for the loss of great crested newt ponds/habitat (either on-site or through District Level Licensing), replacement roosts for bats (either through bespoke features or integrated within new built form) and greenspace for wildlife in general.
- Services across the site including localised high pressure gas mains in the northern and western parts of the site, water mains in the north and a high-pressured sludge main in south.
- Existing Public Rights of Way across the site.
- Two Grade II Listed buildings - Lower Whittle Farmhouse and Brick Farmhouse.
- Long views available to Scout Moor and Holcombe Hill. Retention of such views should be considered where feasible to maintain the connection of the site to the wider landscape.
- Noise and air quality considerations from the proximity of the M62 and M66 motorways.
- The relationship with surrounding existing uses on the site and adjacent to the site.
- Minimise the effects of the development on residential properties situated within the site and the surrounding area considering their setting and views.



Fig. 14: Constraints Plan

-  Allocation Boundary
-  South Heywood Application Area
-  Existing Development
-  Indicative Highway Improvement Area
-  Peat (Indicative Location)
-  Golf Course
-  Agricultural Land
-  Playing Fields
-  Noise constraint Associated with Motorway Traffic
-  Existing Road
-  Public Rights of Way
-  Topography
-  High Retention Value Trees and Hedgerow (CAT A) and Associated Development Offset
-  Moderate Retention Value Trees and Hedgerow (CAT B) and Associated Development Offset
-  Tree Preservation Order (TPO)
-  Indicative Location of Established Hedgerow (CATB - Moderate Retention Value)
-  LHP Gas Line & Easement
-  Water Trunk Mainline & Easement
-  Water Distribution Mainline & Easement
-  Sludge Main
-  Mine Shaft
-  Woodland Habitat Area
-  Woodland Riparian Corridor
-  Priority Habitats
-  Surface Water
-  Floodzone 2 & 3
- Designated Heritage Assets**
  -  Conservation Area
  -  Grade II Listed Building





## Opportunities

- Creation of a new brand and identity for the area/region.
- Employment led development with a strong sense of place to attract a wide range of business sectors and employment opportunities for all. The development will create a range of socio-economic benefits and add social value.
- High-quality market and affordable housing range.
- Sustainable design and construction through low carbon, resource efficient building design and the use of innovative energy technology to ensure the development is resilient to climate change now and in the future.
- New Community Infrastructure in the form of a new school and sports pitches at South Heywood and new areas of retail and other ancillary facilities.
- Creation of additional green infrastructure corridors throughout the site which enhance habitat connectivity on site and to neighbouring habitats off-site.
- Blue/green infrastructure opportunities along Whittle Brook, Castle Brook and Brightley Brook for movement, recreation and enhanced biodiversity as well as sustainable drainage.
- Extend tree planting along the motorway corridors to serve a double purpose of enhancing landscape and visual amenity and promoting wildlife corridors.
- Areas of public recreation along Whittle Brook, Castle Brook and Brightley Brook with connections to Pilsworth fisheries and associated leisure offer.
- Improved highways infrastructure at nearby local and strategic junctions.
- Improved transport infrastructure through a range of modes including potential introduction of Bus Rapid Transit linking to local communities and travel hubs and circular bus routes around Northern Gateway based on quality bus corridor principles. Links to proposed key regional transport schemes such as Metrolink extension to Middleton, Rochdale - Heywood - Bury Tram/Train and Bus Rapid Transit connections to/from Manchester city centre.
- Improved Public Right of Way network with additional high-quality walking and cycling routes links throughout the Site to maximise site permeability and off-site connections to surrounding neighbourhoods.
- Develop a management plan for the site to promote green and blue infrastructure, nature conservation assets, habitats and wildlife which provides health benefits to workers and residents as well as creating a visually attractive environment.









# Key Development Principles

This chapter illustrates how an understanding of the site, its constraints and opportunities, has informed a design approach which has result in the illustrative Development Framework Plan contained within this NGDF.



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Atom

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Ville



## 06. Key Development Principles

This Chapter illustrates how an understanding of the site, its constraints and opportunities, has informed a design approach which has result in the illustrative Development Framework Plan contained within this NGDF.

**6.1** The Design Approach and Development Framework Plan integrate the key influences and objectives identified within the strategic context and local planning policy framework (Chapter 2) and analysis of key issues and opportunities (Chapter 6). They expand upon and provide clarity on how our Vision and Strategic Objectives (Chapter 3) should be applied spatially as part of a joined-up and comprehensive Development Framework.

**6.2** Development at the site will need to be delivered across a range of different land ownerships and through multiple planning applications. The NGDF and supporting Development Framework Plan is therefore intended to help to promote the delivery of a cohesive development across the entire site and avoid fragmented or piecemeal development and should be seen as a positive tool to help facilitate and expedite the planning and development process. Developers and their design teams should consider the design approach when bringing forward proposals for the site.



**The NGDF and supporting Development Framework Plan is therefore intended to help to promote the delivery of a cohesive development across the entire site and avoid fragmented or piecemeal development and should be seen as a positive tool to help facilitate and expedite the planning and development process.**

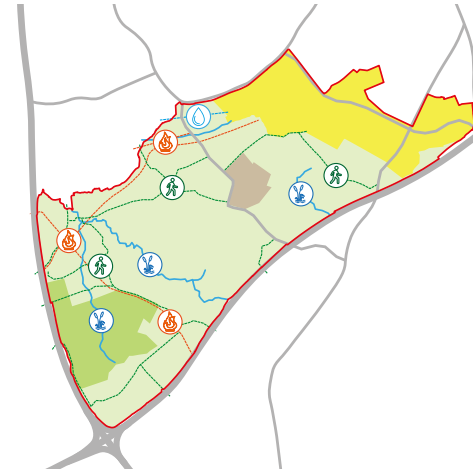


## Design Approach



### Step 1: Comprehensive Approach

**6.3** A comprehensive approach to the site considers the entire area within the allocation boundary as a single comprehensive entity. This approach has allowed for an illustrative Development Framework Plan to be developed in order to fully utilise and maximise the site's development potential.

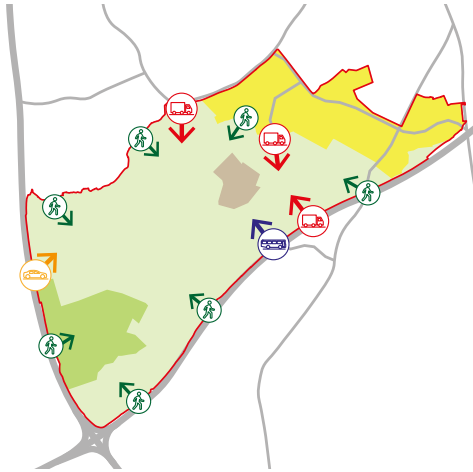


### Step 2: Spatial Constraints

**6.4** A thorough analysis and plotting of the site's known physical constraints has been undertaken in order to recognise the spatial impact they have on the Development Framework Plan. This ensures that the design response is both site specific and responds to the site's constraints, ensuring a more sensitive and efficient approach towards addressing site characteristics and attributes.

**6.5** Site characteristics and known physical constraints, outlined in the previous chapters, include utilities, green and blue infrastructure, heritage assets and ground and mine-workings as well as surrounding land uses.

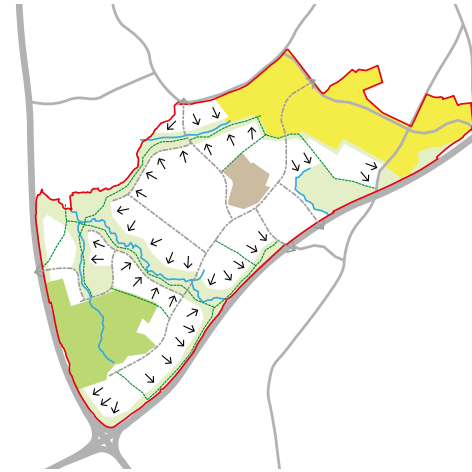




### Step 3: Access

**6.6** Appraising the existing points of access into the site and identifying potential additional points of access ensures opportunities to maximise levels of permeability and integration into the surrounding urban context are fully utilised. Current, proposed and potential future access points into the site have been identified and categorised as follows:

- Points of access suitable for all vehicular traffic.
- Points of access suitable for road-based traffic but with limitations on some types of traffic to limit impacts on surrounding communities, and
- Points of access suitable for users, including pedestrians, cyclists and horse riders, free of road-based traffic and prioritising safe access in the site.

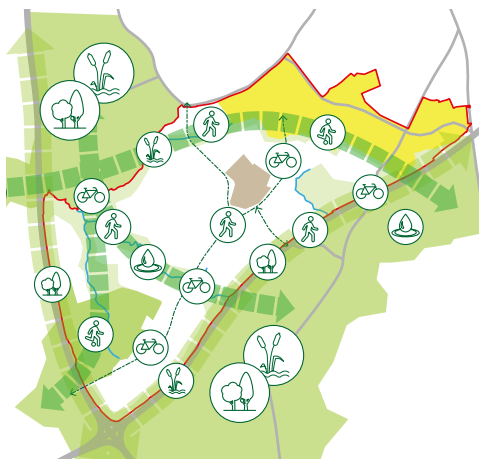


### Step 4: Consolidation and Movement

**6.7** Maximising the development potential of the site is achieved by consolidating - where possible - constraints and any proposed mitigation into a number of corridors through and around the site. This should help free up the type of large and flexible development plots required for large-scale employment uses associated with a site like Northern Gateway.

**6.8** Access to these plots is then provided via a comprehensive and legible hierarchy of routes throughout the site to ensure vehicles, pedestrians, cyclists and horse riders are directed conveniently, efficiently and safely to their destinations. The movement network should accord to the following principles:

- Prioritising the movement of all users, including pedestrians, cyclists and horse riders over that of road-based traffic to ensure the delivery of an accessible, safe, enjoyable and healthy environment for those who use the Northern Gateway site.
- Ensuring development can be delivered in a phased approach and utilises as many points of access as possible in order to connect into surrounding transport corridors.
- Maximising permeability for all to integrate the site into the surrounding movement networks and minimising "dead-end" development.



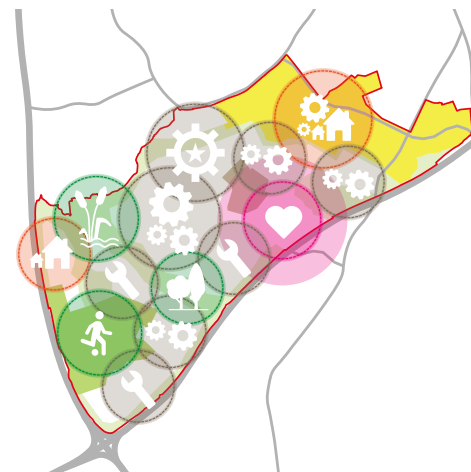
### Step 5: Landscape

**6.9** Adopting a 'landscape-led' approach at the site should ensure that development maximises its potential to deliver meaningful and significant benefits to surrounding communities and the wider region whilst also ensuring that negative impacts are mitigated wherever possible.

**6.10** Implementing a site-wide network of connected green spaces and corridors will help to achieve the following:

- The creation of significant levels of high quality, meaningful and accessible recreation land for proposed and existing employees and residents.

- Retention and enhancement of existing green assets where possible and integration within a series of structural green and blue landscaped corridors. Connected corridors can support local habitats and a cohesive wildlife movement network.
- A variety and extent of open spaces across the site allows for increased biodiversity and natural landscape value, meaningful sport and recreation benefit to local communities, promote health and well-being of residents and employees.
- The delivery of naturalised mitigation measures as a priority including drainage, noise and visual mitigation, and ecological mitigation and Biodiversity Net Gain (BNG).



### Step 6: Use and Gateways

**6.11** Proposals at the site should fulfil the site's role as a national-scale and strategically important employment-led development. Opportunities should therefore be explored to ensure development at the site is vibrant, safe, sustainable and enjoyable through the introduction of supporting and complementary uses to ensure the site creates its own clear identity. These include:

- Maximising the potential associated with a development of this scale and critical mass, through the establishment of central Mixed-use Hubs which provide a range of uses and facilities which should support proposed employment uses and act as a focal point to the wider development.

- Delivery of complementary housing provision, local centre, and other ancillary supporting uses located at key gateways.
- Ensuring a diverse range of different employment uses are catered for - such as Advanced Manufacturing - and are clustered in appropriate areas.
- Ensuring all uses are in a location and setting which is appropriate for that use whilst not negatively impacting on the delivery of surrounding development.
- Ensuring development is compliant with Policy JP Allocation 1.1 of Places for Everyone (2024).

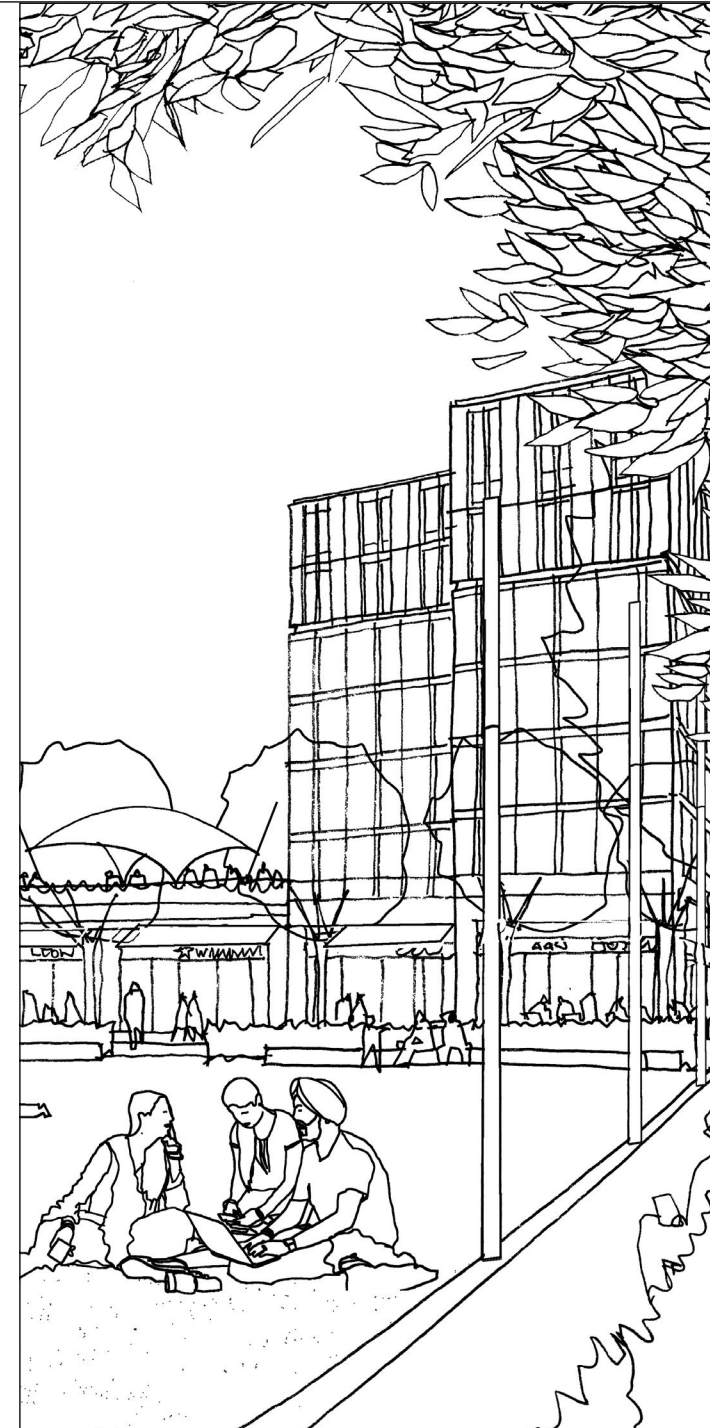


### Step 7: Active Travel

**6.12** Promoting a modal shift towards more sustainable forms of transport and promoting healthier transport options including walking and cycling is a central element in the delivery of sustainable development. Ensuring development prioritises sustainable travel is therefore key.

**6.13** A site-wide Active Travel Strategy should be adopted across the Northern Gateway site which prioritises the following:

- A permeable network of routes across the site able to accommodate a range of different bus routes allowing for the diversion of existing routes into the site as well as accommodating the needs of new bus routes in the future.
- The identification of a network of integrated 'Transport Hubs' and 'Transport Nodes' across the site enabling safe, easy and secure interchange between bus, cycle, scooter and walking, with a hub being accessible within a 10-minute walk of all parts of the site where possible in order to create a human scale and walkable development.
- Delivery of a comprehensive, site-wide Active Travel network providing accessible, safe and enjoyable routes for all.





## Development Framework Plan

**6.14** The Development Framework Plan is a culmination of the above Design Principles and is an indicative interpretation of how these design principles can be realised spatially across the Northern Gateway site.

**6.15** It establishes the principal spatial components of development at Northern Gateway, including the extent of indicative development areas, land uses, areas of strategic open space, key access points and primary movement routes, and demonstrates how a high-quality, sensitive and site-specific response based around urban design best-practise can be delivered on the site.

**6.16** The key elements of the Development Framework Plan are as follows:

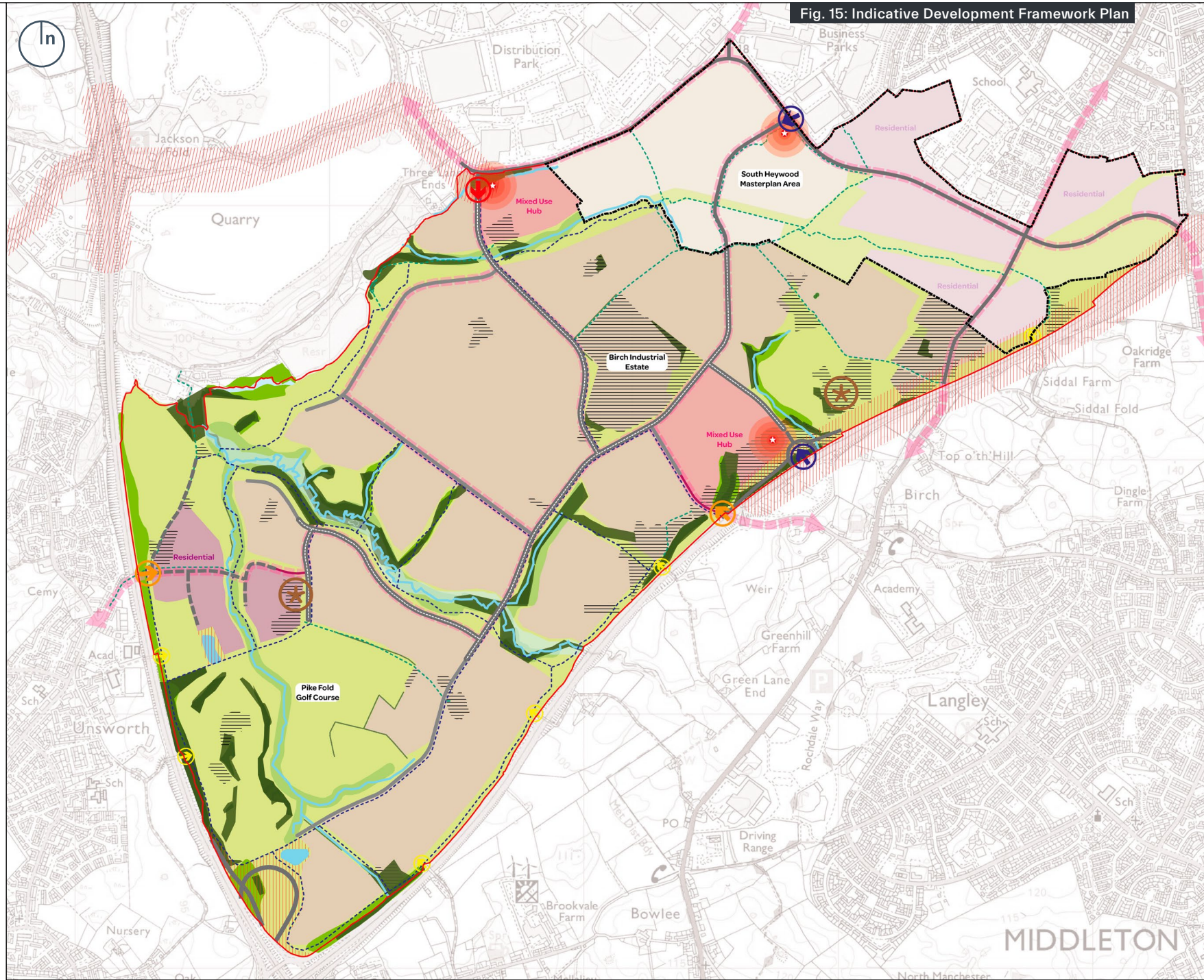
- Delivery of around 1,200,000 sqm of high-quality employment space suitable for a range of employment uses including advanced manufacturing, industrial, storage and distribution, as well as around 1,200 new homes, consisting of around 1,000 new homes at South Heywood and around 200 new homes off Castle Road, alongside other ancillary uses.
- Delivery of a permeable, safe and legible network of routes across the site creating a highly accessible and sustainable development which fully integrates into surrounding movement and transport infrastructure and promotes sustainable transport choices.
- Delivery of a structured network of green and blue routes and spaces helping to provide a meaningful and accessible recreation resource to proposed and surrounding communities, helping to support and enhance local habitats and biodiversity, and provide naturalised mitigation helping limit any negative impacts the development of Northern Gateway may have on its surrounding context.

**6.17** This Development Framework Plan is illustrative in nature and should be considered as such. It is intended to support the NGDF as an illustrative but realistic interpretation of how the design principles contained within this NGDF could be interpreted and delivered to help guide and inform future proposals for the site.

**6.18** The Development Framework Plan is not technically exhaustive in its approach and is based on a number of assumptions and goals which may change or evolve during the delivery of the project. Consequently, there may be other acceptable ways that the PfE policy requirements and the design principles contained within the NGDF can be interpreted and implemented which are in full accordance with the SPD but which may differ spatially from this Development Framework Plan.

**6.19** The Development Framework Plan is based on a reasonable and the most up to date understanding of the site and its constraints available at the point of drafting this NGDF. Flowing from the Development Framework Plan are a series of plans relating to specific aspects of the Development Framework Plan.

- / Allocation boundary
- Potential developable area
- Existing built development
- South Heywood Application Area
- Green open space
- Woodland
- Woodland Riparian Corridor
- Woodland/Priority Habitat
- Watercourse
- ★ Listed Building (Grade II)
- Indicative Highway Improvement Area
- ↑ Proposed Access Point (all traffic)
- ↑ Proposed Access Point (restricted traffic)
- ↑ Potential Access Point (all traffic)
- ↑ Proposed Access Point (pedestrian / cycle access)
- Primary Road (all traffic)
- Secondary Road (all traffic)
- Residential Road (no HGV)
- Bus Road (bus only)
- Retained Footpath
- Relocated/New Shared Path
- Employment uses
- Residential uses
- Mixed-use
- ★ Gateway location
- Bus enabled route





## Development Framework Principles

### Built Form and Uses

**6.20** The NGDF delivers high-quality employment space suitable for a range of employment uses as well as around 1,200 new homes alongside other complementary and supporting uses to proposed and surrounding communities. The estimated 1,200,000 sqm of employment floorspace should attract a variety of growing and emerging business sectors including (but not limited to) advanced manufacturing, logistics, industrial, data centres, storage and distribution. The delivery of around 1,200 new homes, with a range of types, sizes and tenures, including affordable housing, should be supported by community infrastructure including a primary school, sports pitches, a local retail centre and public green space. The majority of the residential component (up to 1,000 dwellings) and up to 175,460 sqm of employment space is already consented and in the process of being delivered as part of the approved South Heywood Masterplan. In addition to South Heywood, around 200 further homes should be located to the west of the site along Castle Road.

**6.21** Key to the Northern Gateway Vision is the delivery of a dynamic and thriving innovation hub for advanced materials and manufacturing, as well as the opportunity to embrace the site's scale in order to accommodate a potential large or strategically significant 'giga-scale' employer.

**6.22** In order to create a vibrant and dynamic employment ecosystem where businesses and communities interact and thrive, advanced manufacturing uses should be clustered together. The smaller scale form of buildings associated with this use are ideally suited for areas along linear parks and near existing development where they are less likely to have a negative impact on their surrounding context. An Advanced Manufacturing Corridor to the north of Birch Industrial Estate has therefore been identified allowing opportunities for this key use to be delivered in the initial phases of the wider Northern Gateway development, as well as being located in a prominent and highly accessible part of the site.

**6.23** The Development Framework Plan has also been developed to accommodate a number of large plots which provide the flexibility required for large format industrial buildings. Plots to the east of Whittle Brook could be merged into a single plot in the event a 'giga-scale' investment opportunity becomes available.

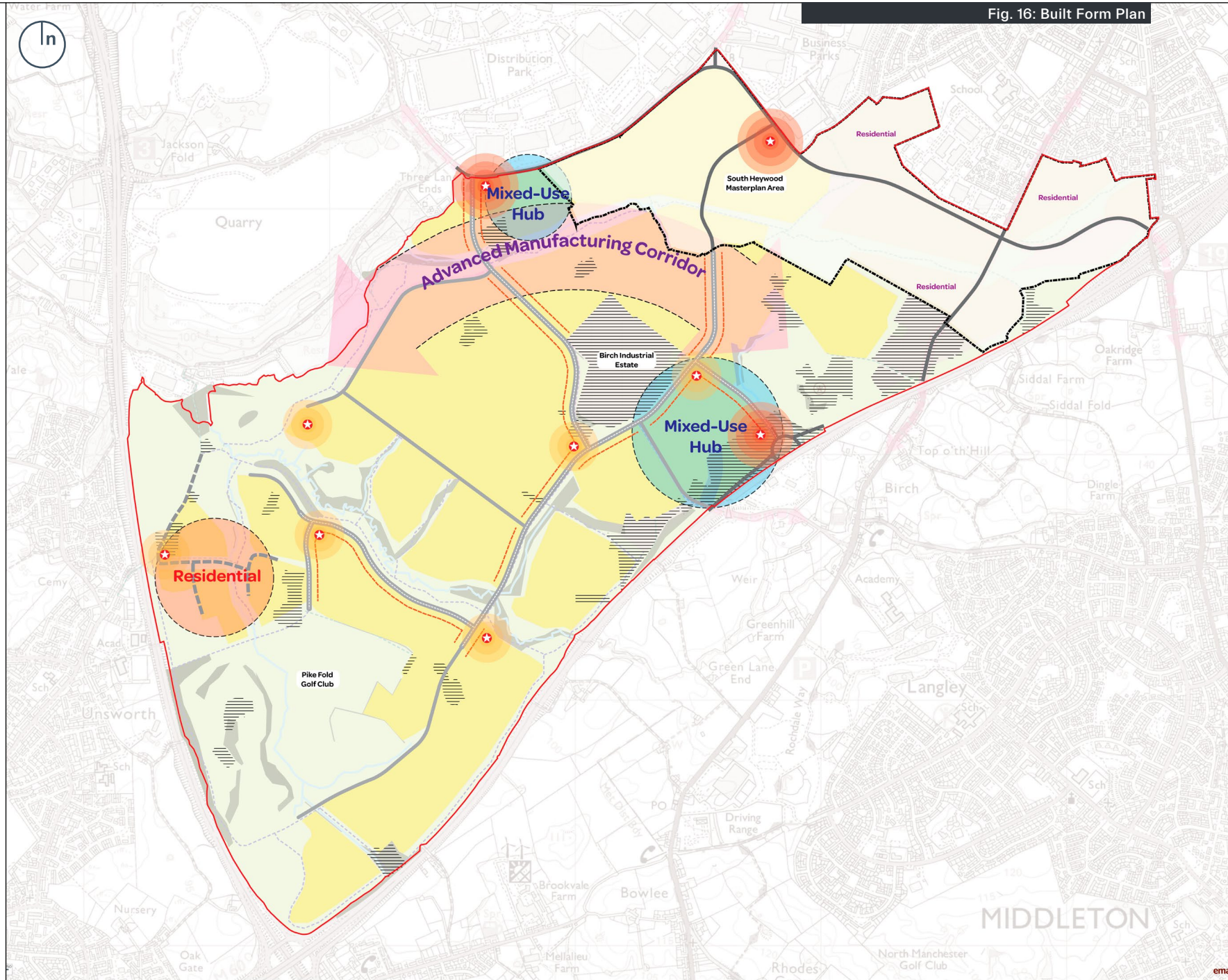
**6.24** The site should feature distinctive architecture influenced by the site's vernacular strategically placed at key locations including key junctions and nodes, defining the development as a striking destination with a strong identity. These architectural features should contribute to branding Northern Gateway and the wider region. Active building uses should be clustered at gateways, transport nodes and along key routes and spaces throughout the development, ensuring buildings have a positive relationship with surrounding streets and public spaces where possible, contributing to a safe environment. Imaginative architectural solutions to large format industrial buildings should be explored including materiality, boundary treatments, increased visual permeability and dynamic lighting to better respond to surrounding context.

**6.25** Mixed-use Hubs should act as the primary nodal points within the site. Mixed-use Hubs should contain a range of uses, potentially including hotel and conference facilities, a gym and creche and other related and complementary uses which deliver a vibrant meeting place at the heart of the surrounding employment uses. The Development Framework Plan currently identifies potential locations for these Mixed-use Hubs.



Fig. 16: Built Form Plan

- / Allocation boundary
- Potential developable area
- Existing built development
- Primary Routes
- Secondary Routes
- Employment uses
- Employment uses (Advance Manufacturing Focus)
- Mixed-uses
- Residential uses
- \* Gateway location
- \* Key location/junction
- / Key route frontage



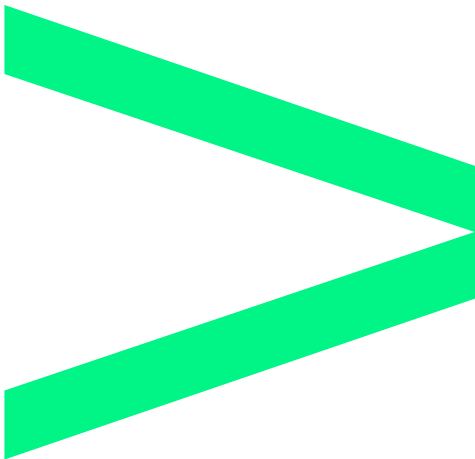
**6.26** The proposed larger Mixed-use Hub adjacent to the M62 would also include a re-imagined off-line MSA to replace the existing Birch MSA which is likely to be removed in the event a new access into the site is created in this location. In the event a new access off the M62 is not delivered or considered viable, land adjacent to the northern site access (Pilsworth Road/Moss Hall Road Junction (south)) would become the main Mixed-use Hub within the site with the existing Birch MSA being retained.

**6.27** These Mixed-use Hubs will be supplemented with Transport Interchanges due to their location close to main entrance points into the site and other proposed public transport/active travel routes. Their architectural qualities and design should reflect the area's important role as both a primary hub and gateway.

**6.28** The South Heywood Local Centre is another key Mixed-use Hub within the site. This Mixed-use hub is designed principally to provide convenience retail and a primary school for the consented South Heywood development but would also provide local facilities for the wider allocation, particularly in terms of the public transport and active travel connections.

**6.29** Other key locations/junctions, including the Castle Road entrance to the site and key views should be defined by increases in building scale and architectural qualities to improve legibility as well as the potential for complementary uses.

**6.30** Northern Gateway aspires to be one of the most sustainable employment locations in the country. Best practice standards in sustainable design and construction should maintain the aspiration to deliver a zero-carbon development through resource efficient building design and the use of innovative energy technology.



## Movement and Access

**6.31** The site's location immediately adjacent to the M62, M60 and M66 Motorways is one of Northern Gateway's greatest assets with regard to its accessibility and connections to the surrounding region.

**6.32** Two proposed access points have been identified, one at the Pilsworth Road/Moss Hall Road Junction (south) to serve the initial phases of the employment development and one at Castle Road which will be restricted to accessing the residential development and public transport/active travel links to the wider employment development. In the event that the northern site access is not sufficient to accommodate the full quantum of development which is proposed, additional access point(s) will be needed to provide extra capacity, as well as permeability through the site and to provide resilience on the highway network. Additional access points will also provide additional routes for public transport and active travel and will ensure the site is attractive to all potential investors.

**6.33** Improvements to the M62 could include upgrades to Junction 19 which could create a new point of access into the site at Birch Services with further improvements seeing additional capacity added to Junction 3 of the M66 and Simister Island Interchange. An access off Queen Elizabeth II Way could also be provided. Various local road network improvements are also likely to be required to improve access and traffic flows.

**6.34** Traffic modelling will determine the precise number of additional accesses required and when they will be delivered.

**6.35** The Development Framework Plan is based around a clear route hierarchy delivering a permeable, safe and legible network of routes across the site. This is based on a vision to create a highly accessible and sustainable development which fully integrates into surrounding movement and transport corridors and promotes sustainable transport choices. These routes have not yet been fixed but would be delivered in accordance with the clear route hierarchy.

**6.36** Key to this vision is prioritising the safe movement of pedestrians, cyclists and horse riders over that of road-based traffic whilst enabling the site to function efficiently as a high-quality employment site. Streets and public spaces should be designed to be accessible to all, facilitating people to become more active in their everyday lives. Building orientation and design also being considered to maximise levels of activity and natural surveillance over streets and spaces. The design and layout of buildings should also promote Active Travel, with entrances direct and open, and cycle and car parking easily accessible and walkable.



**6.37** All streets across the site should be designed in accordance with Transport for Greater Manchester's (TfGM) Streets for All guidance as well as LTN 1/20 principles.

**6.38** Promotion of a modal shift towards more sustainable forms of transport and healthier travel habits should also be a priority. Key to this will be a network of 'Transport Hubs' and 'Transport Nodes' across the site connected with a range of active travel links and routes. The scale and form of such Hubs and Nodes are yet to be determined but should provide access to a range of bus connections as well as safe and secure access to bike storage and change facilities, e-bike and e-scoots rental, and potential opportunities for park and ride to fully utilise first-mile and last-mile connectivity principles. The distribution and location of hubs and nodes should ensure as much of the site as possible is located within a 10-minute walking distance of either facility helping to create a walkable and accessible environment for all. Primary Transport Interchanges should be located within Mixed-use Hubs and be accessible to all bus routes passing through the site to surrounding transport hubs, whilst offering regular connections into surrounding Transport Nodes via local bus, cycle and walking connections. Transport Nodes should be located at key locations including the South Heywood Local Centre and Castle Road as well key junctions and nodal points across the site.

**6.39** Where feasible, existing Public Rights of Way should be retained and upgraded in-situ, however it is expected that some routes may need to be diverted to facilitate future development. Likewise, new active travel connections will be required to mitigate the severance between existing communities to the west and south of the site caused by the M66 and M62 motorways. All routes must create comfortable, universally accessible, coherent, attractive and safe routes. Where possible, footpaths and cycle routes should be located within generous landscaped corridors thus promoting local opportunities for active recreation.

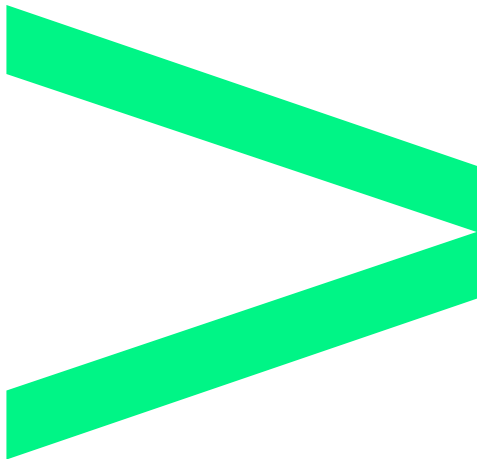

















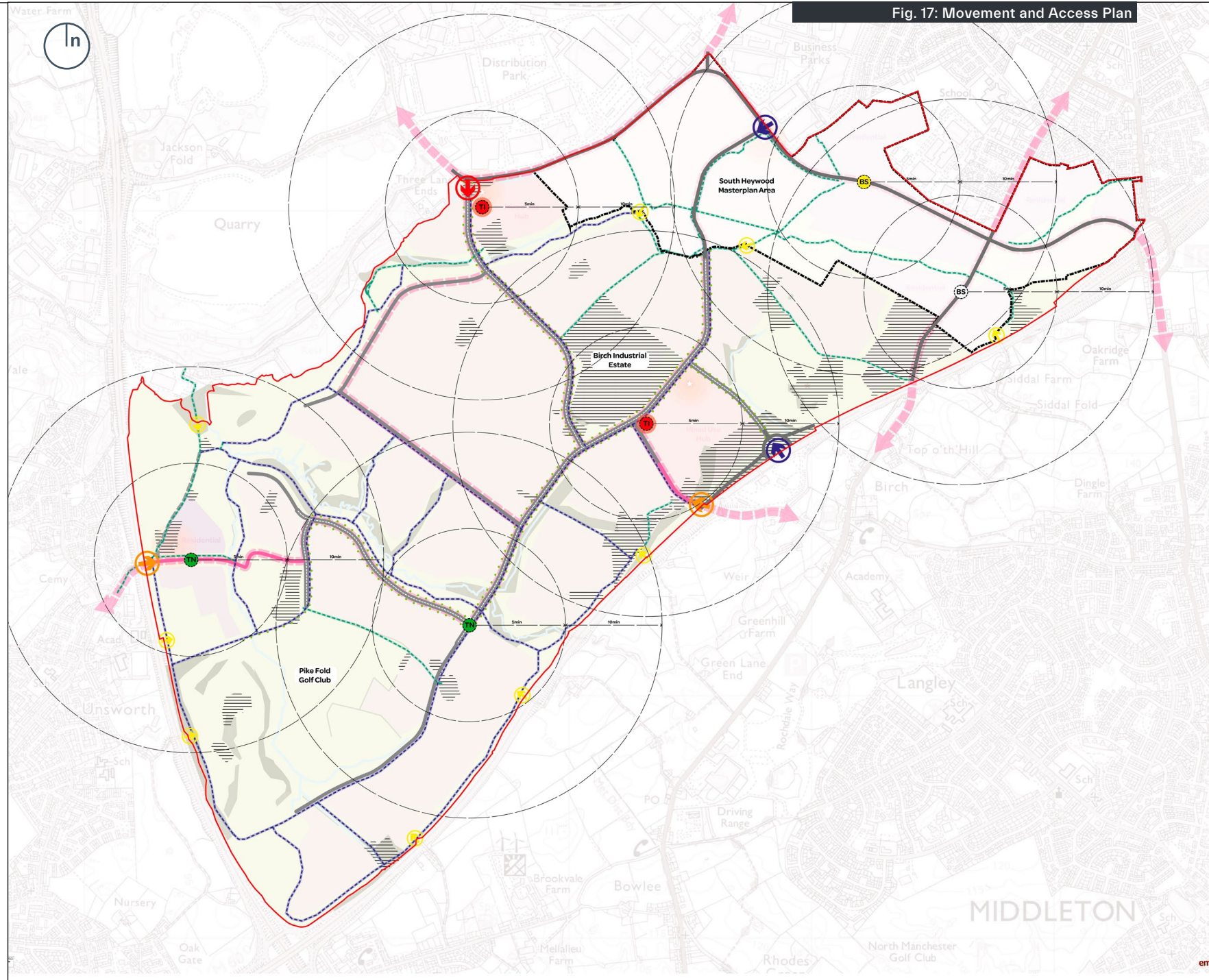


Fig. 17: Movement and Access Plan

-  Allocation boundary
-  Proposed Access Point (all traffic)
-  Proposed Access Point (restricted traffic)
-  Potential Access Point (all traffic)
-  Proposed Access Point (pedestrian / cycle access)
-  Primary Road (all traffic)
-  Secondary Road (all traffic)
-  Residential Road (no HGV)
-  Bus Road (bus only)
-  Retained Footpath
-  Relocated/New Shared Path
-  Bus enabled route
-  Primary "Travel Interchange"
-  Secondary "Travel Node"
-  Existing Bus Stop
-  Proposed Bus Stop (Indicative location)
-  5/10 min walking distance



## Landscape and Public Realm

**6.40** A landscape-led approach to the Development Framework Plan should see the site contained within a high-quality parkland setting offering multiple benefits: for biodiversity and natural landscape; for sport and recreation; for sustainable water management and the health and well-being of employees, residents and the wider community. Opportunities to work with natural processes and adopting nature-based solutions as part of the wider approach to managing surface water and drainage should be utilised. The site should open up substantial areas of public open space for all to enjoy. These should include a range of different typologies including formal managed areas of landscaping for amenity, sport and recreation, as well as naturalised areas of landscaping to support local wildlife, biodiversity and habitat creation across the entire site.

**6.41** Green and blue infrastructure networks across the site should be formed through the retention and enhancement of identified environmental features of the existing landscape. Valuable existing natural assets, such as the brooks and their u-shaped valleys, mature trees, hedgerows and woodland blocks should be retained where possible and integrated within a series of structural landscaped corridors to deliver overall gain for the environment. Formal and naturalised landscaped corridors should connect to public open space within the approved South Heywood Masterplan, Pilsworth Reservoir to the north and green spaces within the surrounding local area to ensure development at the site is incorporated successfully into the local landscape setting.

**6.42** Connected structural landscaped corridors should protect the areas of highest ecological value and form a cohesive wildlife movement network which enhances habitat connectivity on site and to neighbouring habitats off-site. Ecological improvements should include the retention

and enhancement of the Whittle Brook, Castle Brook and Brightley Brook and associated riparian habitats as well as ponds, areas of woodland in the site and alongside the motorway edges. Ecological improvements should also see the installation of new attenuation basin wildlife ponds, extensive tree, hedgerow and scrub planting and enhancement of existing grassland.

**6.43** The NGDF promotes and prioritises the delivery of naturalised and habitat friendly mitigation measures for drainage, land re-profiling, noise and visual matters over standard engineered solutions. Where possible, underground water drainage and storage should be avoided with the use of consolidated naturalised attenuation basins and open swales being encouraged. Engineered acoustic fences and built retaining features should also be discouraged over tree planting, earth bunds and landscaped reprofiling for site mitigation. This approach should help maximise opportunities to deliver areas of habitat and recreational value whilst also reducing ongoing maintenance burdens.

**6.44** The Development Framework Plan identifies indicative locations, many of which represent significant stretches of land for example the proposed corridor located along the route of the Whittle Brook, which would allow for the delivery of some ecological mitigation measures required to achieve Biodiversity Net Gain (BNG) and these will be delivered in a phased manner as development comes forward. Given the scale of development across the allocation, it is likely that significant off-site mitigation will also be required to achieve BNG. Multi-functional green space will not always be considered suitable for mitigation aimed at achieving BNG.

**6.45** Areas of landscaping, habitat creation and recreation should not be limited purely to areas of open space. Opportunities should also be explored to utilise streets, pedestrian and cycle routes and bridleways for the creation of green, blue and ecological corridors. Verges, planted boundary treatments and screening should also be utilised to contribute towards the sites wider Green Infrastructure. Features such as street running swales, rain beds and native habitat friendly shrub planting should be encouraged within the design of streets. Seating and rest areas should also be integrated into the design of streets to help promote active travel and support those with limited mobility.

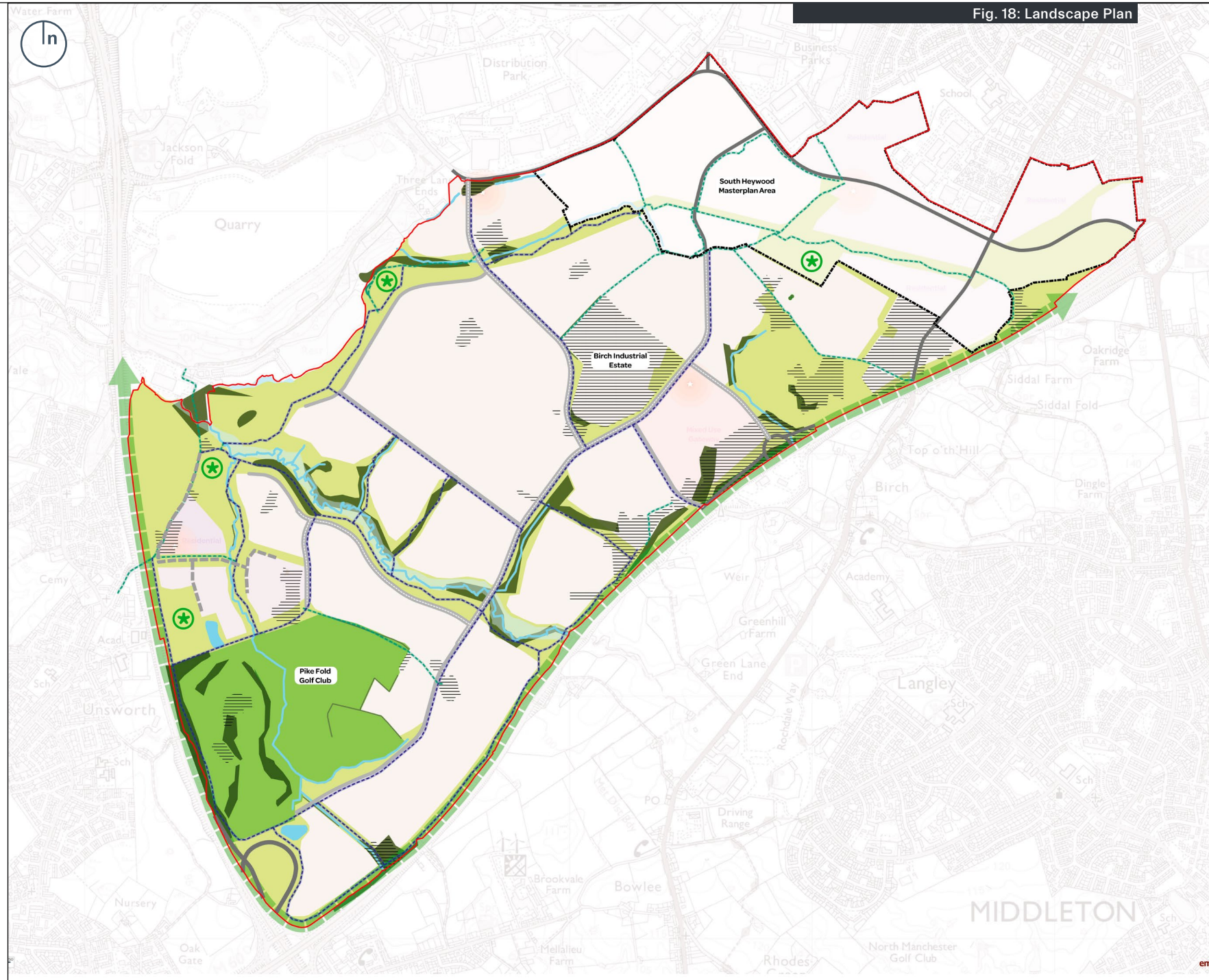
**6.46** The amalgamation of the built form within this ecologically rich, biodiverse setting should create a high quality and distinctive parkland destination focused on community interaction and sustainable movement. New areas of public open space embedded throughout the site could feature sports facilities, recreation hubs, walking and running trails, cycling routes, and event spaces, enriching the surrounding environment with diverse opportunities for leisure and activity.

**6.47** The site should prioritise the provision of high-quality public realm providing safe, accessible and attractive streets and public spaces with a strong identity, character and a sense of place. The public realm should be activated through a range of uses to promote interaction and recreation. Measures to deter anti-social behaviour should be integrated into the design, employing both active and passive solutions such as activity, surveillance, lighting, strategic planting and appropriate materials.



Fig. 18: Landscape Plan

- Allocation boundary
- Potential developable area
- Existing built development
- Green open space
- Woodland
- Mitigation corridor
- Existing Golf Course
- Watercourse
- Water body
- Retained Footpath
- Relocated/New Footpath & Cycle Route
- ⊛ Existing recreation space
- South Heywood open space



# Design Principles and Character Areas

The Development Framework Plan demonstrates how Northern Gateway can be delivered, operate, and function



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## 07. Design Principles and Character Areas

The Development Framework Plan demonstrates how Northern Gateway can be delivered, operate, and function in a spatial form that is policy compliant and technically sound. However, when implementing the vision for the site as a comprehensive, enjoyable and safe place which is unique to the site's attributes, it is key to explore the character and appearance of a place as well as how it functions.

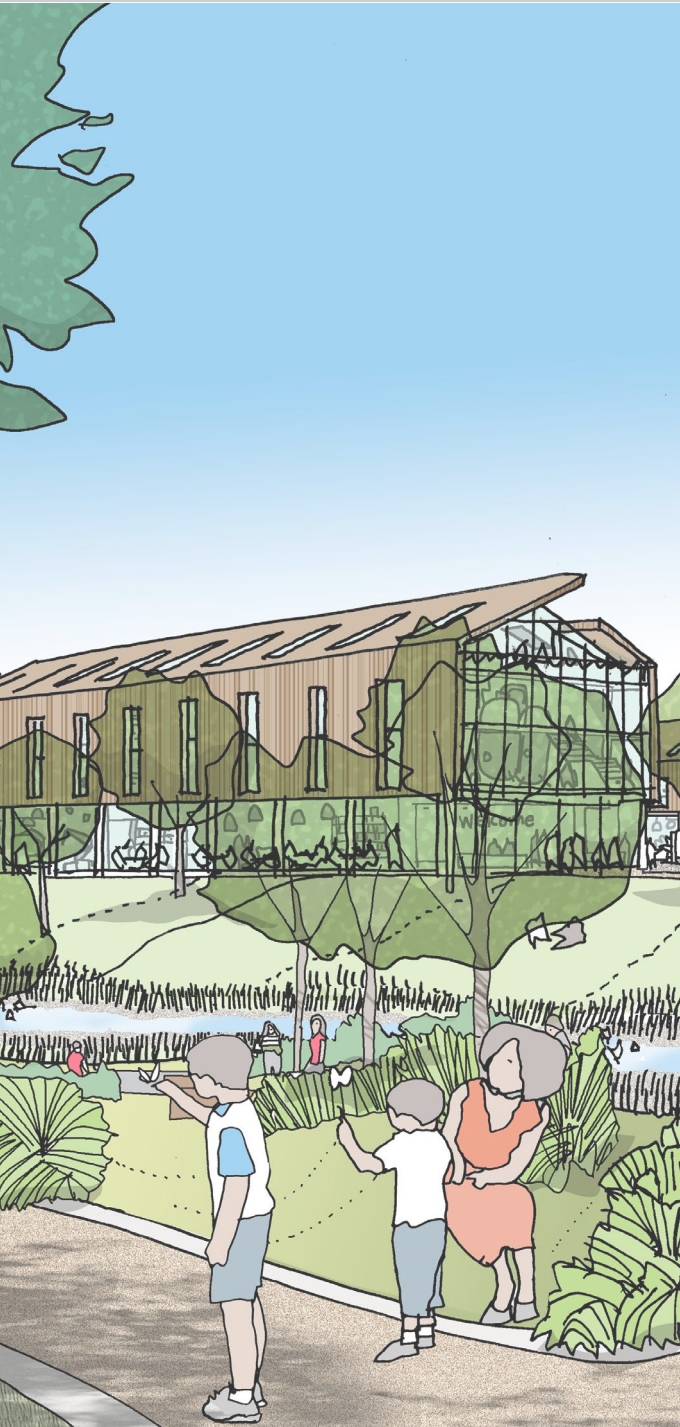
7.1 With a site as large and complex as Northern Gateway, it is necessary to consider design principles in both a site-wide context as well as principles which are specific to only certain part(s) of the site. Future proposals for the Northern Gateway site need to be specific to both the wider Northern Gateway site as well as the specific design considerations relevant to individual parts of the site. When taking into consideration the number of constraints which need to be considered and the number of varying uses which will be accommodated on the site, some parts of the site will likely have completely different characteristics and functions compared to other parts of the site. This may be in response to key site attributes, constraints or proposed uses. These different areas are referred to as Character Areas.

7.2 The following chapter will discuss a range of design principles in both a site-wide and Character Area context.









**7.3** The following site-wide design principles should be applied to all future development proposals within the Northern Gateway site regardless of their location with any given Character Area.<sup>4</sup>

### Built form and Use

- Striking building forms, design and materiality should help create a distinctive destination with a clear identity and contribute to branding Northern Gateway and the wider region.
- Large format industrial buildings in key prominent locations should embrace innovative elevation design solutions in a way which breaks up their visual form and reduces their impact on the surroundings.
- Ensure building design and form provides a positive interface with proposed adjacent uses and spaces, as well as responding to existing constraints and characteristics.
- Ensure building typologies create a human scaled and comfortable environment with positive relationships with public spaces.
- Office elements, primary entrances and other active uses within buildings should face primary routes and public realm where possible for improved legibility and to maximise levels of natural surveillance and perceived safety and security.
- Build forms, scale, use and appearance should positively respond to gateway locations, key junctions and nodes where possible.

4. These design principles do not apply to the South Heywood Masterplan Area which is subject to its own Design Code.



## Streets and Movement

- Primary Routes running through site should take the form of a boulevard incorporating segregated foot and cycleways as well as tree planted verges to ensure routes provide safe and enjoyable Active Travel Corridors which prioritises the movement of non-road based users as well as fulfilling their function to serve surrounding proposed uses.
- Secondary Routes running through the site should take the form of green streets incorporating segregated foot and cycleways as well as suitable landscaping to ensure routes provide safe and enjoyable Active Travel Corridors which prioritises the movement of pedestrians, cyclists and horse riders as well as fulfilling their function to serve surrounding proposed uses.
- All streets should be designed in accordance with Manual for Streets (MfS) and Streets for All principles.
- Vehicle carriageways should be reduced in width at pedestrian and cyclist crossing points and along long stretches of road to help reduce traffic speeds and create a safe and attractive crossing point and prioritise the movement of pedestrians and cyclists. These should be marked with changes in surface materials and increased levels of tree/shrub planting.
- Where feasible, consolidation of parking should be explored to ensure efficient use of land and maximise space given to other features. Large areas of car parking should be located away from streets wherever possible, or include high levels of formal landscaping and suitable boundary treatments to break up large areas of hardstanding and create a positive interface and allow for natural surveillance with and over adjacent pedestrian/cycle routes.
- Service and good yards should be located away from streets wherever possible or be screened from public view through provision of suitable levels of landscape planting so not to have a negative impact on surrounding streets.
- All pedestrian and cycle routes throughout the site should be design to be accessible to all and well-lit to provide safe and easy movement for all throughout daytime and evening hours.

## Landscape

- Ensure provision of a high-quality public realm providing safe, accessible and attractive public spaces with a strong identity, character and a sense of place.
- Green and blue infrastructure networks across the site should be formed through the retention and enhancement of identified environmental features of the existing landscape. Valuable existing natural assets should be retained where possible and integrated within a series of structural landscaped corridors to deliver measurable net gains for biodiversity.
- Retention of existing vegetation of value and considered within the parcel layout where possible.
- Tree and hedgerow planting to be included in all car parks to break up the visual mass of vehicles and create an overall consistency of urban greening within the development.
- Carriageway structure to include sufficiently sized landscaped verges for tree lined streets and naturalised linear swales or raingardens to manage rainwater run-off.
- Delivery of naturalised and habitat friendly mitigation measures for drainage, land re-profiling, noise and visual matters to be prioritised over standard engineered solutions.
- The location and optimisation of nature-based solutions and holistic SUDs measures should be informed by a site wide drainage strategy which then identifies strategic interventions for each character area.

## Character Areas

7.4 Each Character Area responds to its unique physical setting and reflects key attributes from the surrounding area. This helps to create distinct and memorable places within the proposed development with its own clear identity whilst helping to improve legibility and improve navigation around such a large development. Whilst they may look, feel and operate differently from each other, it is important they tie-in together in other ways to ensure the Northern Gateway development as a whole appears as a single comprehensive development.

7.5 The Development Framework Plan for Northern Gateway can be divided into six Character Areas as follows:

- Mixed Use Hub
- The Boulevard
- Core Development
- Urban Valley
- Boundary Corridors
- Castle Road

7.6 Each Character Area narrative below introduces the roles and high-level design aspirations for each Character Area and sets out key design principles which will help to guide future proposals for the site.

7.7 The objective behind this approach is to provide a coherent and cohesive design language across the entire site which is specific to the area and its existing character, and which allows the scheme to read as a single entity, whilst allowing future proposals to respond to key opportunities within the site, including gateways, improving legibility, providing suitable uses.

7.8 Character Areas do not have defined boundaries and commonly overlap. Larger development plots within the Development Framework Plan may include two or more Character Areas. In such situations, proposals should consider how the different Character Areas vary and ensure future proposals respond accordingly, especially along the boundaries of plots and their relationship with surrounding routes and open space.

7.9 The following section clearly set out how this should be achieved through a range of design principles, precedent images and indicative sketches to help inform future proposals. These are not designed to be exhaustive and are intended as high-level design principles on which future applications should be based.

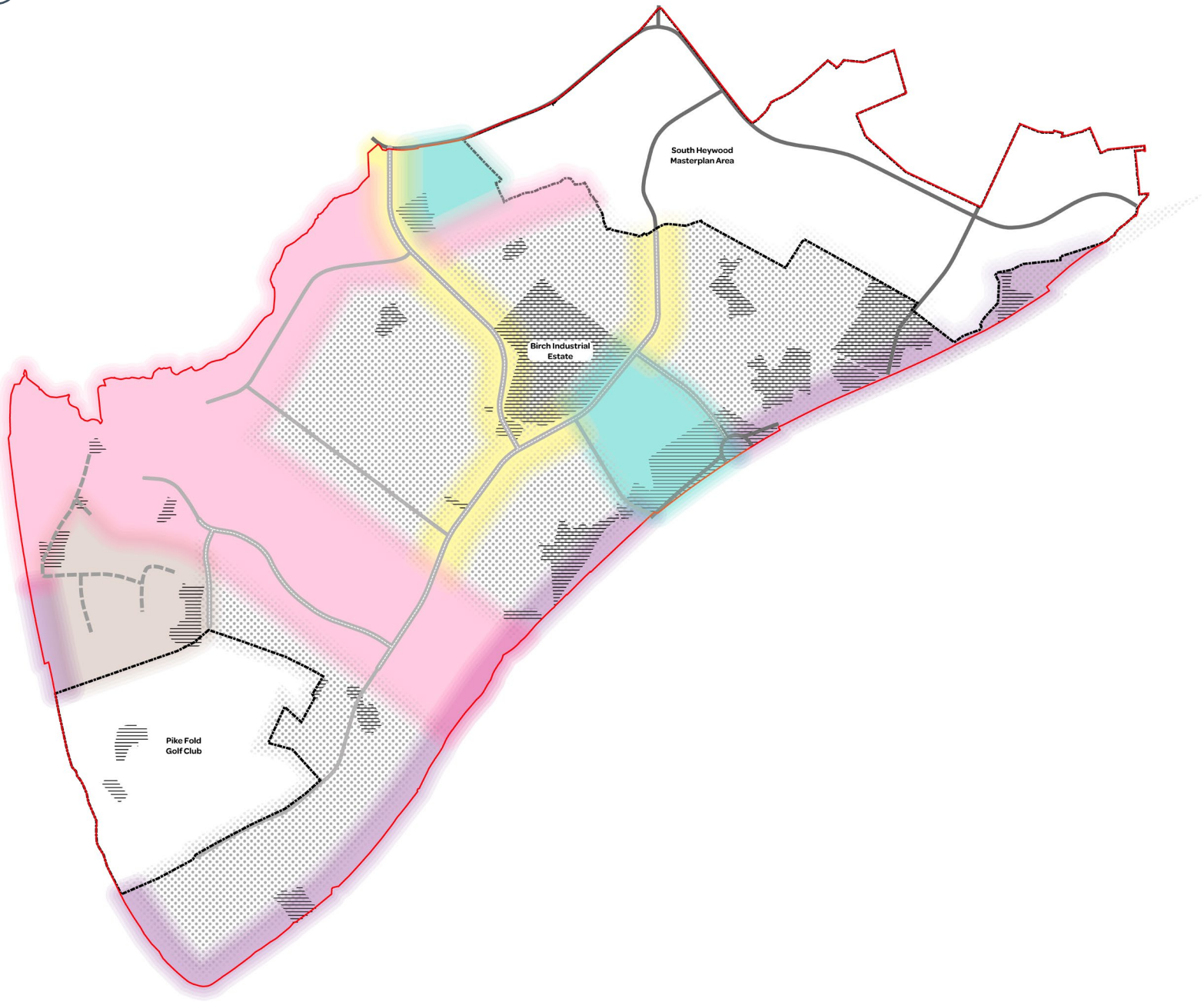
7.10 Further detail supplementing these principles should be provided in the form of a detailed Design Code(s) which will need to accompany any future planning applications for development on the site. Such Design Codes should be drafted alongside consultation with officers and key stakeholders and demonstrate compliance with the design principles contained within this NGDF SPD and any other national and local planning and design policies and guidance relevant to that proposal.

7.11 Figure 19 shows the spatial distribution of the Character Areas across the site.

Fig. 19: Character Area Plan

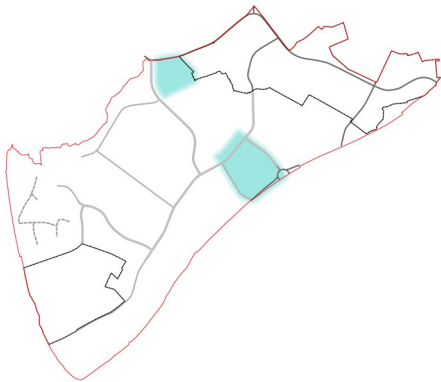


- Allocation boundary
- Mixed-Use Hub
- The Boulevard
- Core Development
- Urban Valley
- Boundary Corridors
- Castle Road
- Existing built development





## Character Area 1 – Mixed Use Hub



**The Mixed-Use Hub  
Character Area serves  
as a primary point of  
arrival and a central  
focal point and meeting  
place for Northern  
Gateway.**

7.12 The Mixed-use Hubs are located in prominent and highly accessible parts of the site near primary access points. The largest proposed hub is located to the south of the site and is defined by the M62 to the south and Birch Business Park to the north, a re-aligned Whittle Lane and Birch Farm marking the eastern and western edges, respectively.

7.13 This location has been determined due to the baseline assumption and long-term vision that a new vehicular access point will be delivered providing direct access from the Northern Gateway site on to the M62 Motorway which will act as a primary gateway into to the site. As part of this process, the existing Birch MSA will need to be removed and re-provided within the proposed Hub.

7.14 A second smaller Mixed-use Hub is located at the northern site access (Pilsworth Road/Moss Hall Road Junction (south)). It is designed to supplement the larger hub and be delivered within the initial phases of the Northern Gateway development. In the event the Mixed-use Hub at the northern site access is delivered and a new access off the M62 is no longer considered viable, the Pilsworth Road/Moss Hall Road Mixed-use Hub will act as the site's primary hub. In this scenario, the Birch MSA would be retained in situ.

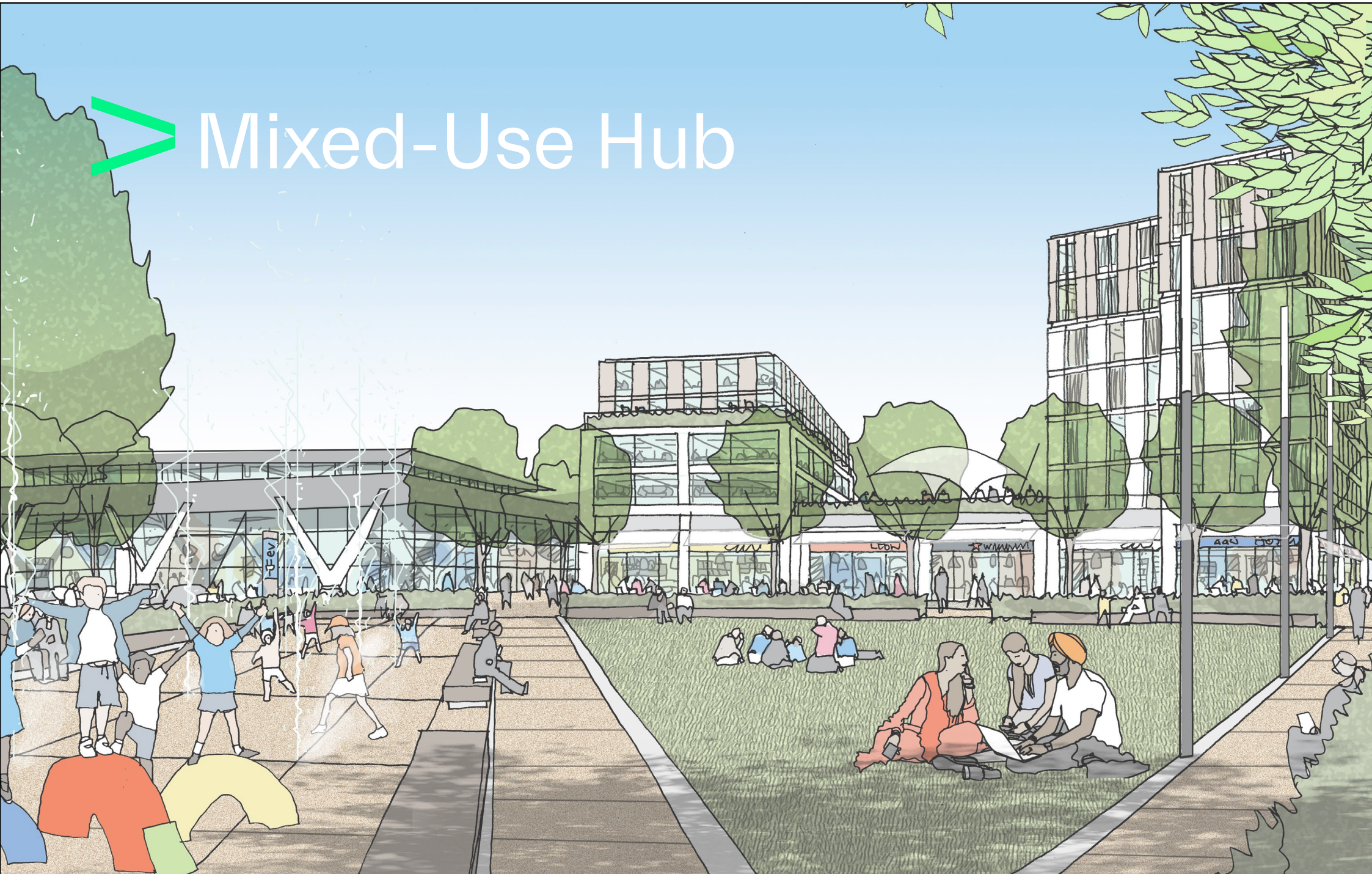
7.15 The Mixed-use Hubs serve as the primary points of arrival and as central focal points and meeting places for Northern Gateway. These areas will include Transport Interchanges which will provide both regular connections to wider Greater Manchester and the potential for regular shuttle bus connections to secondary travel nodes within Northern Gateway. The Transport Interchanges will also connect to active travel routes through the site.

7.16 The Mixed-use Hubs will support the surrounding uses and compliment the proposed South Heywood Local Centre. They should contain a range of ancillary uses which could include a hotel and conference facilities, retail and commercial, a gym and creche and other related active uses to contribute to the delivery of a vibrant meeting place at the heart of surrounding employment uses.

7.17 Both the architectural and public realm design should create a high-quality, distinctive destination with a clear identity and branding to reflect the character area's role as the primary centres of Northern Gateway. Highly prominent plots are to be utilised by key uses with increased building scale and activity to create a striking gateway entrance into Northern Gateway. The Mixed-use Hubs should also promote human scaled features, have a high-quality and active public realm and benefit from green and blue infrastructure. An emphasis on comfort, relaxation, wellbeing and human need should have commercial benefits with visitors wanting to spend longer here.



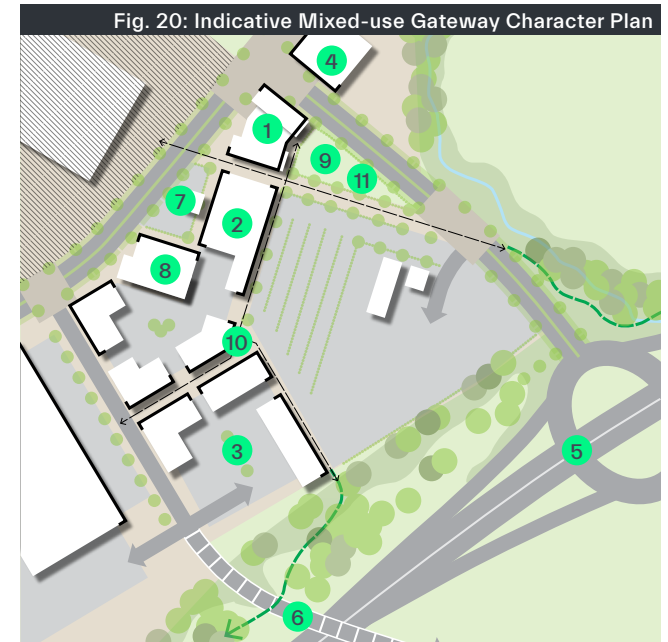
# > Mixed-Use Hub





## Mixed-use Hubs Key Design Principles

1. Maximise opportunities for distinctive architecture to reflect Mixed-use Hubs role as primary centre within the development and gateways for the wider development.
2. Mixed-use Hubs should include provision for a range of potential complimentary and ancillary uses to the wider Northern Gateway development including provisions for a new off-line MSA (southern Mixed-use Hub only), hotels and conference facilities, gym and creche facilities and other supporting retail/commercial uses.
3. Additional uses to be considered within the Mixed-use Hubs could include smaller format and higher density employment uses and other complementary uses such as office, research, start-up, co-working and educational uses which can help complement the vibrant character of the Mixed-use Hubs and utilise the areas high quality public transport links and accessibility.
4. Increase building scales to improve local legibility and have visual presence at key arrival points.
5. Potential highway improvements to M62 Junction 19 to establish a new primary point of entry into the site from the M62 Motorway. (southern Mixed-use Hub only)
6. Highway improvements to Whittle Lane to establish a new Active Travel route into the site connecting on to Heywood Old Road. (southern Mixed-use Hub only)
7. Allow for the provision of Transport Interchanges accessible to all bus routes passing through Northern Gateway and regular connections to wider transport hubs within Greater Manchester.
8. Primary Transport Interchanges should provide regular shuttle bus connections to Secondary Travel Nodes within Northern Gateway as well as alternative Active Travel transport options such as e-scooters and e-bike rental and potential opportunities for an integrated parking facility/multi-storey car park to support potential park and ride services.
9. Include provision for the delivery of a formal recreation space or square including potential for seating, picnic space, play, water feature and relaxation which can act as a focal point to the Mixed-use Hubs and for use by and accessible to all.
10. Routes and spaces through and around the Mixed-use Hubs should be designed to be safe and well-lit throughout daytime and evening hours, allow suitable areas for shelter from wind and whilst also allowing for outdoor seating/dining from surrounding food and beverage uses.
11. Areas of formal open spaces should be designed to be adaptable and suitable for a range of small-scale temporary uses including pop-up events and retailers.

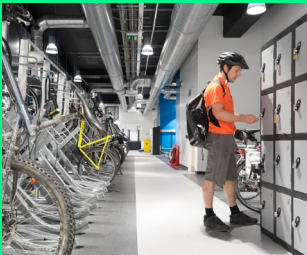




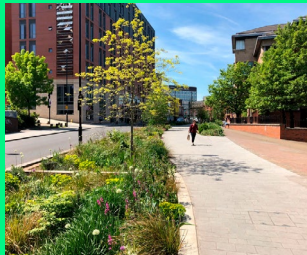
Built Form and Use



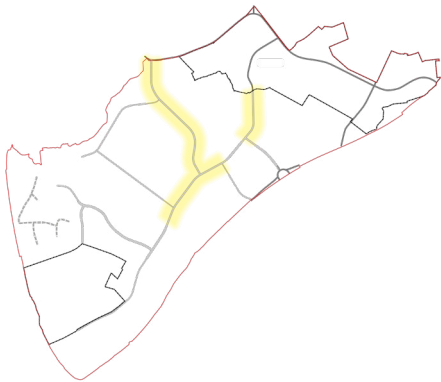
Streets and Movement



Landscape



## Character Area 2 – The Boulevard



7.18 The linear configuration of The Boulevard Character Area aligns along the Primary Routes throughout Northern Gateway and is primarily focused on land to the east of Whittle Brook.

7.19 The Boulevard Character Area creates a highly prominent and accessible corridor that acts as the primary axis through the development and will form a spine for those moving through and within Northern Gateway. The Boulevard Character Area accommodates all vehicular traffic, supports sustainable public transport, and promotes active travel modes while establishing a high-quality public realm. Primary Routes running through The Boulevard incorporate Active Travel Corridors with segregated footpaths and cycleways and tree planted verges, ensuring these routes are safe and enjoyable and prioritise pedestrian and cyclist movement.

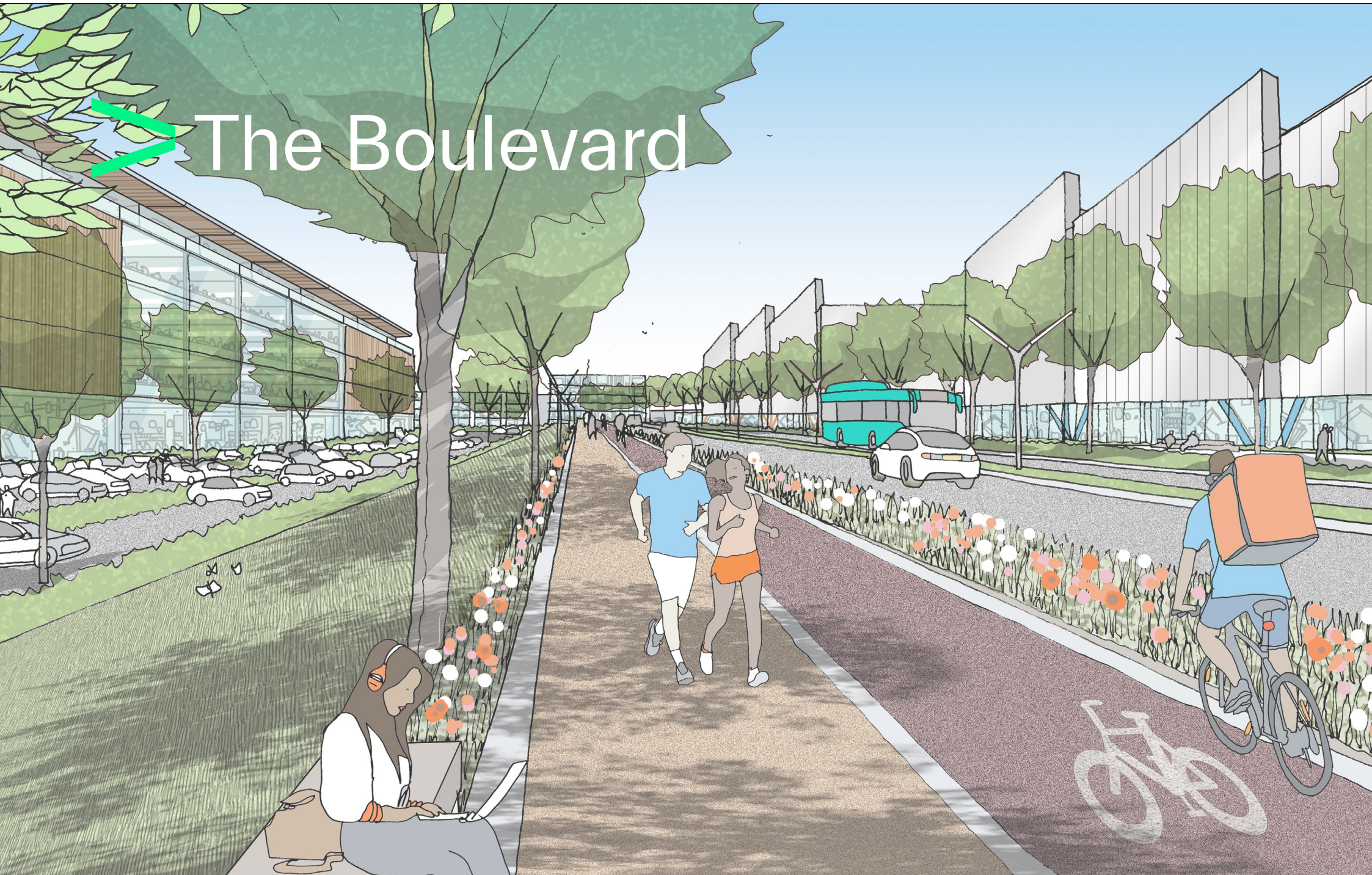
7.20 Roads and streets should be designed in accordance with MfS principles, whilst accommodating movements for large vehicles where appropriate. Carriageway widths should be as narrow as possible throughout. Crossing points should be appropriate for the anticipated speed, volume and composition of motor traffic. It is likely that the safest type of crossing would be controlled crossings.

7.21 Along the Primary Route, a series of secondary gateways, key junctions and nodes introduce variation to the streetscape. Highly visible building facades in prominent locations to include innovative elevational solutions where possible, varying their forms, scales, uses, and appearances to aid orientation. The Boulevard Character Area includes the frontages of employment buildings situated along the Primary Route. Office elements, primary entrances, and other active uses within these buildings are designed to interact positively with the Primary Route and the surrounding public realm.



**The Boulevard Character Area creates a highly prominent and accessible corridor that acts as the primary axis through the development.**



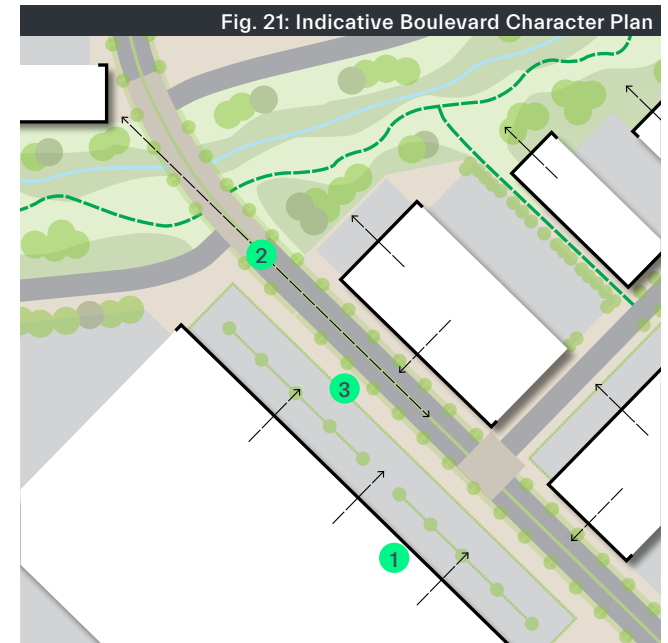


# > The Boulevard

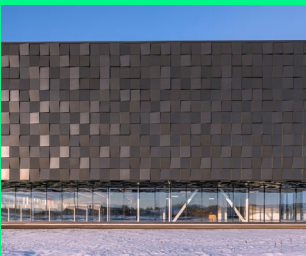


### The Boulevard Key Design Principles

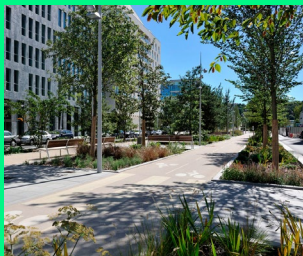
1. Ensure building design and form responds to the role of The Boulevard as the primary movement corridor through the site and will therefore be highly visible to those moving through and within the wider development.
2. Secondary Travel Nodes should provide regular shuttle bus connections around the wider Northern Gateway site as well as provision for alternative Active Travel transport options such as e-scooters and e-bike rental and potential opportunities for park and ride.
3. Boundary treatments along primary streets should be formal, low level and include high quality landscaping to ensure opportunities to maximise level of natural surveillance between surrounding uses and the street frontage. High level and visually impermeable boundary treatments along primary streets will only be permitted when screening service yards and should be screened by landscaping features.



Built Form and Use



Streets and Movement

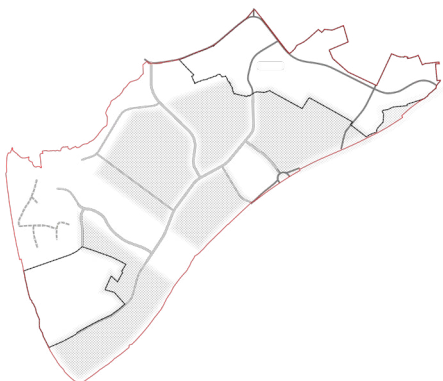


Landscape





### Character Area 3 – Core Development



7.22 The Core Development Character Area spans the entire site, encompassing all available land outside the other four other Character Areas. Consequently, this Character Area is divided into a series of smaller areas.

7.23 The Core Development Character Area serves as the functional employment element of Northern Gateway and provides the greatest flexibility for delivering development for a variety of growing and emerging business sectors. The Core Development Character Area aids the delivery of a dynamic and thriving innovation hub for advanced manufacturing, as well as the opportunity to embrace the sites scale in order to accommodate a potential ‘Giga-Scale’ employer alongside logistics, industrial, storage and distribution sectors.

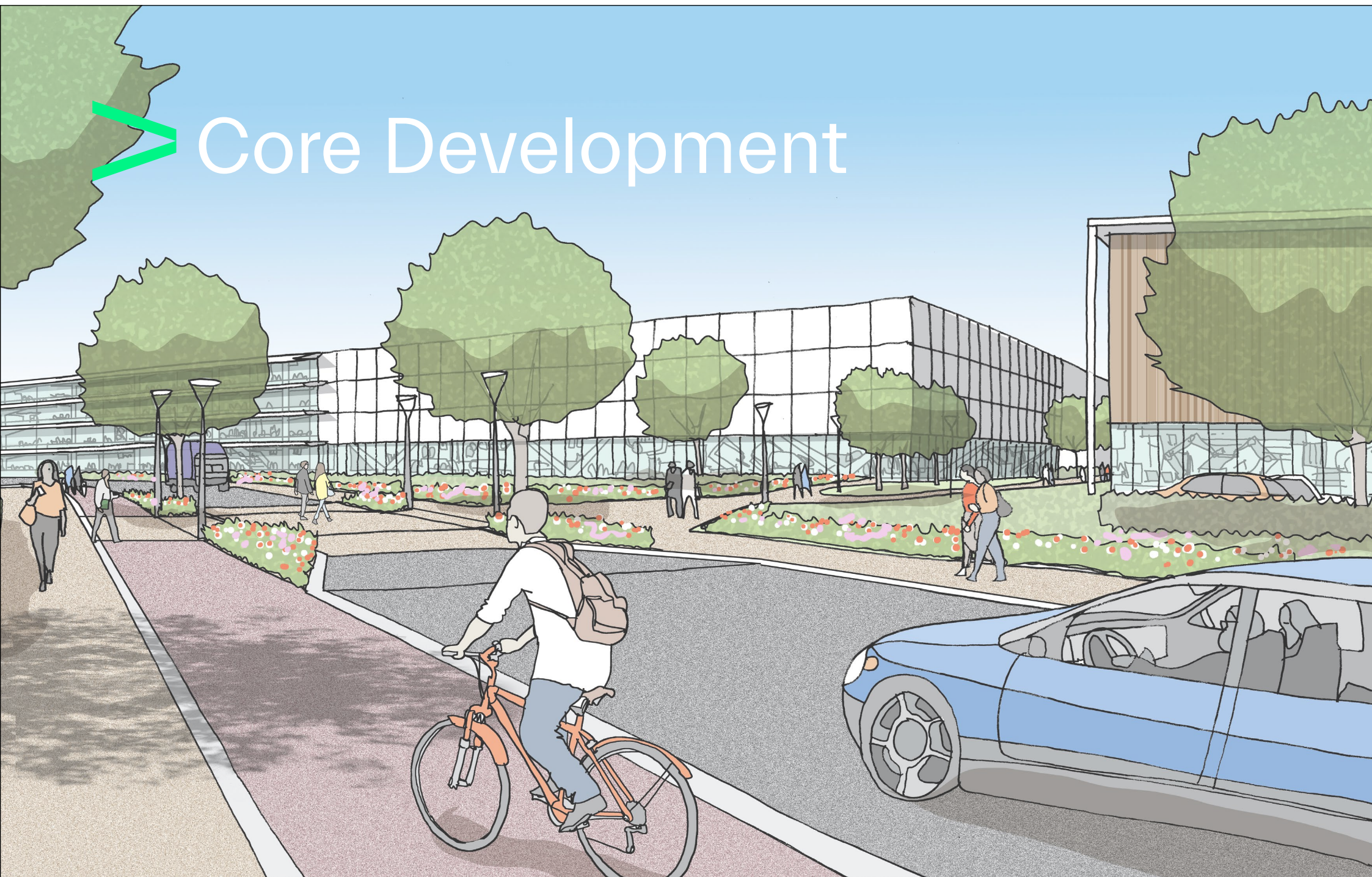
7.24 Secondary Routes running through the Core Development Character Area take the form of green streets incorporating segregated foot and cycleways as well as suitable landscaping to ensure routes are safe and enjoyable Active Travel Corridors. Additional footpaths and cycle routes run between development plots with suitable levels of natural surveillance to deliver a permeable and legible network of routes across the site.



**The Core Development Character Area aids the delivery of a dynamic and thriving innovation hub for advanced manufacturing, as well as the opportunity to embrace the sites scale in order to accommodate a potential ‘Giga-Scale Plus’ employer.**



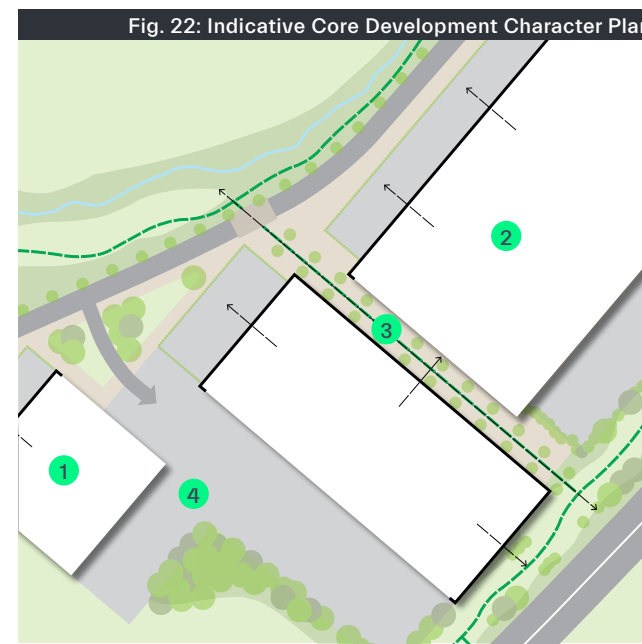
# > Core Development





## Core Development Key Design Principles

1. The design of smaller format Advanced Manufacturing uses should be designed to promote dynamic and thriving innovation hubs for advanced materials and manufacturing. These should include premises of different sizes to attract varied scales of business and allow for the retention of business on-site as they grow. Development should also include a range of communal and co-working facilities to promote sharing, communication and innovation.
2. The design of larger format 'Giga-Scale' should be designed to lessen their impact on the surrounding development where possible. It is possible that the site will attract large occupiers, where self-containment and security is important. In these instances, careful consideration should be given to design to ensure wider permeability with other areas of the site. This should utilise opportunities to integrate in with surrounding complementary uses, enable pedestrian and cycle permeability, and where possible, allow provide/enable shared facilities.
3. Due to the large format nature of employment-led developments such as Northern Gateway, where development allows, opportunities should be utilised to further improve permeability through the site (including links to the consented South Heywood scheme) with the introduction of additional pedestrian/cycle routes between development plots. These should be designed to include suitable levels of natural surveillance, landscape and be accessible to all users.
4. Where possible opportunities should be explored to consolidate parking and introduce shared/communal parking for smaller employment uses/buildings to reduce levels of hardstanding.
5. Areas of soft landscaping, public realm, open space and SuDS should be provided, where possible these should be in naturalised forms in order to maximise ecological opportunities and constraints. These areas have not been identified at this stage in order to maintain flexibility.



Built Form and Use



Streets and Movement

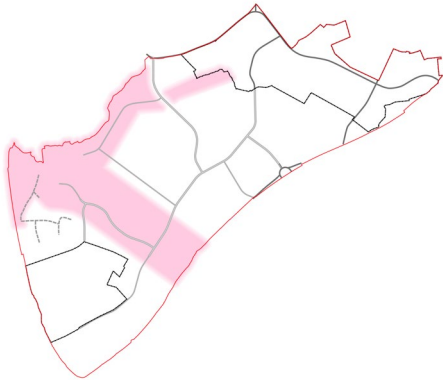


Landscape





## Character Area 4 – Urban Valley



7.25 The Urban Valley Character Area aligns along the watercourses of Whittle Brook and Brightley Brook and captures development within the western extent of the northern boundary and a central region of Northern Gateway on a north-west to south-east axis.

7.26 The Urban Valley Character Area forms a high-quality parkland setting to enable new areas of meaningful and accessible public open space with diverse opportunities for sport and recreation to bring about health and well-being benefits for employees, residents and the wider community. Development here is set within the landscape setting with building form, design and appearance having a positive impact on the area's role. Walking and running trails will meander throughout the Character Area with integrated community spaces and sport provision where compatible with ecological considerations. In connection with the Boundary Corridors Character Area, Whittle Valley protects areas of the highest ecological value and forms a cohesive wildlife movement and habitat network within Northern Gateway as well as connecting to green spaces within the surrounding local area to integrate with the wider landscape setting.

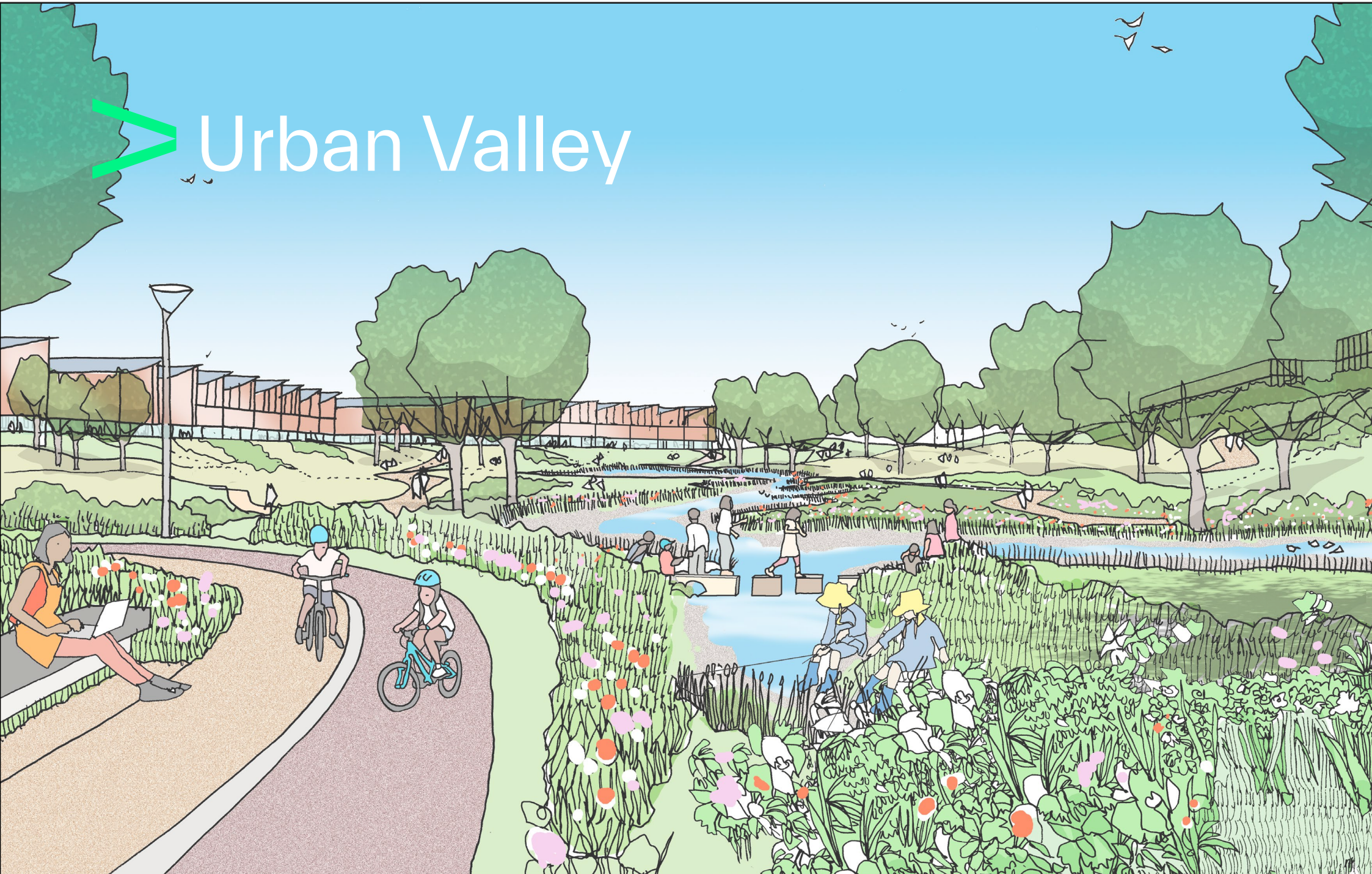
7.27 The existing brooks, their tributaries and u-shaped valleys alongside woodland blocks and riparian habitat are retained and enhanced as far as possible and highlighted as distinguishing and mature landscape features. Supplementary landscape features to include naturalised and habitat friendly sustainable water management, land reprofiling solutions and further soft landscaping.



**The Urban Valley Character Area forms a high-quality parkland setting to enable new areas of meaningful and accessible public open space.**



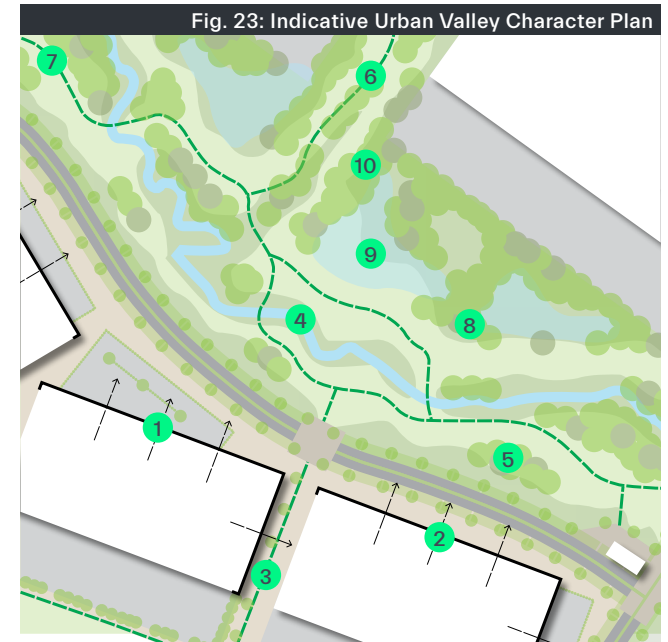
# > Urban Valley





## Urban Valley Key Design Principles

1. Ensure building design and form responds to the role of the Urban Valley Character Area as containing some of the primary area of public open space across the site, as buildings will be highly visible to pedestrians and cyclists moving through and within the wider development.
2. Building elevations which front on to areas of public open space should embrace innovative design solutions in a way which breaks up their visual form, reduces their impacts on surrounding public spaces and maximises levels of natural surveillance.
3. Due to the large format nature of employment-led developments such as Northern Gateway, where development allows, opportunities should be utilised to further improve permeability through the site linking large areas of public open space with surrounding streets with the introduction of additional pedestrian/cycle routes between development plots. These should be designed to include suitable levels of natural surveillance, landscape and be and accessible to all users.
4. Provision of green and blue infrastructure network formed through retention and enhancement of existing natural assets, in particular Whittle Brook, Castle Brook and Brightley Brook, and integration within a series of structural green and blue landscaped corridors.
5. Including landscaped corridors to form a high-quality parkland setting for Northern Gateway offering multiple benefits: for biodiversity and natural landscape; for sport and recreation; for sustainable water management and the health and well-being of employees, residents and the wider community.
6. Parkland setting should enable new areas of high quality, meaningful and accessible public open spaces with diverse opportunities for sport and recreation
7. Landscaped corridors should connect to public open space within the South Heywood Masterplan, Pilsworth Reservoir to the north and green spaces within the surrounding local area to integrate with the wider landscape setting.
8. Landscaped corridors should protect the areas of highest ecological value, increase habitat value with various landscape enhancements and form a cohesive wildlife movement network.
9. Inclusion of naturalised and habitat friendly sustainable drainage systems to manage rainwater run-off from hard paved spaces and buildings.
10. Deliver naturalised mitigation solutions to land reprofiling and retaining features and other site-wide infrastructure requirements.

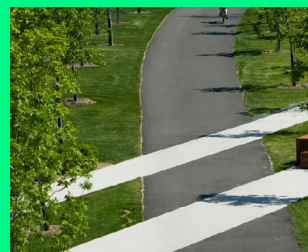




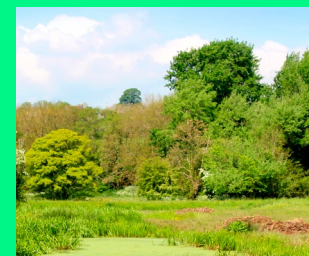
## Built Form and Use



## Streets and Movement

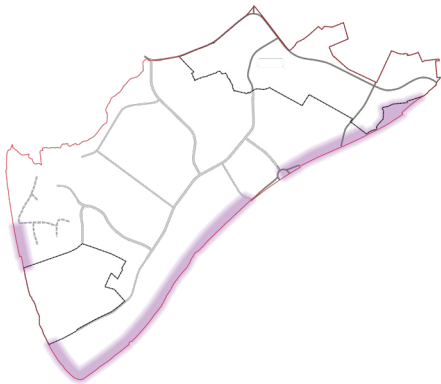


## Landscape





## Character Area 5 – Boundary Corridors



7.28 The Boundary Corridor Character Area bounds the M62 and M66 motorways. Its linear configuration extends along the majority of the southern and western boundary, excluding Pike Fold Golf Course, the existing playing fields and Castle Road proposed residential area.

7.29 The Boundary Corridor Character Area recognises the opportunity to create a multi-functional, high-quality, safe and enjoyable peripheral linear public open space that offers numerous benefits. Rather than merely retaining tree-belts along the periphery of this site, the Boundary Corridor Character Area will instead build off this asset to create a circular traffic free green corridor for use by employees, surrounding communities and local wildlife alike.

7.30 The corridor will connect a number of currently unlinked and poor-quality pedestrian and cycle routes which currently cross the motorways into the site to create a safe, attractive and legible network of routes. Where possible they should seek to enhance the quality of access points across adjacent motorways, improving safety, increasing accessibility for all active modes of transport, and provide attractive local entrance points into the site.

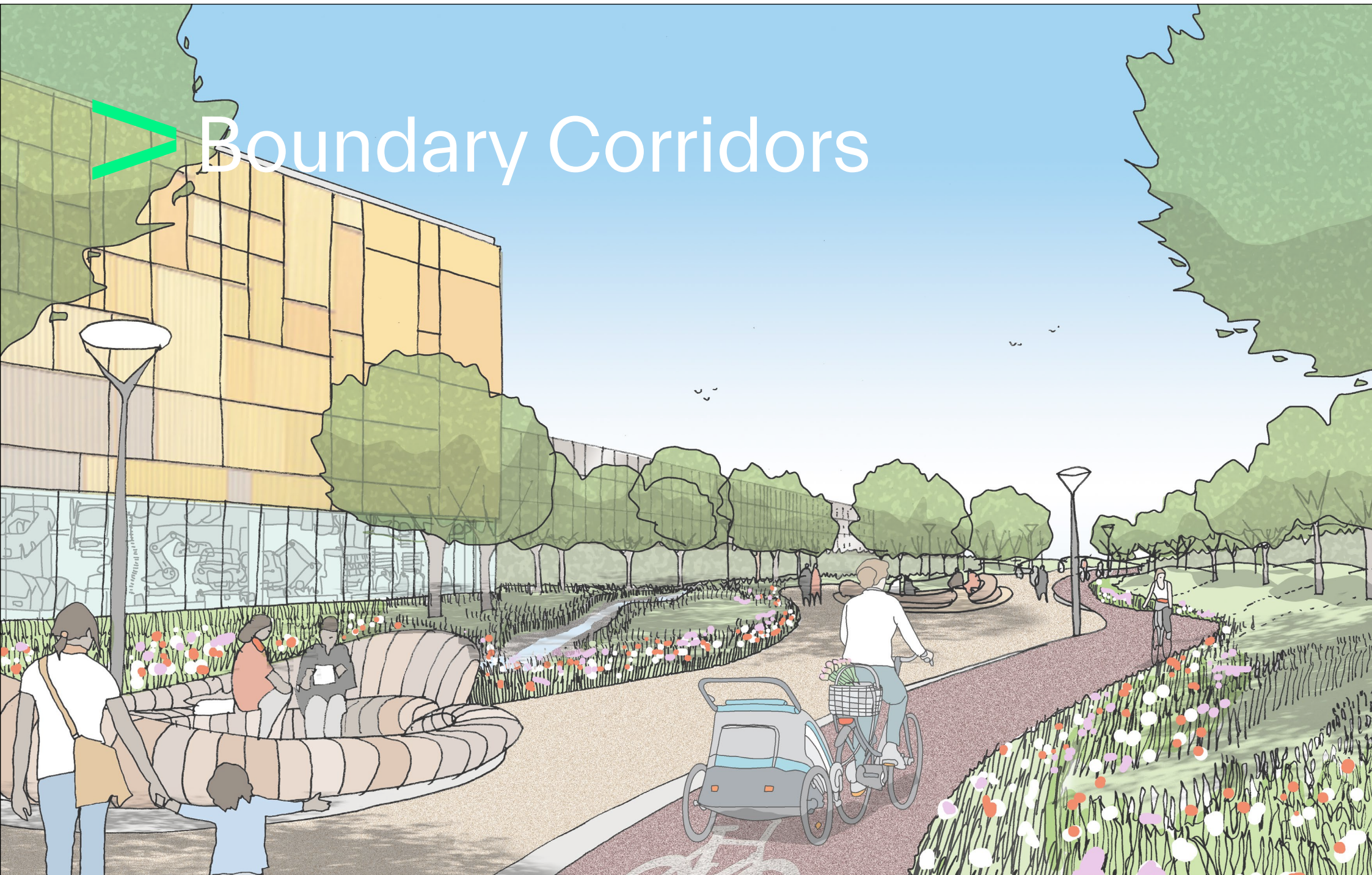
7.31 To reinforce existing tree with additional structural landscape planting, mitigating the impacts of the nearby M62 and M66. For cohesive habitat connection and wildlife movement benefits, the existing and proposed landscaping, combined with naturalised sustainable drainage systems, will strengthen and diversify the current habitats, thereby enhancing the ecological value of the site. For sport and recreational benefits, a perimeter pedestrian and cycle active travel network provides car-free travel routes, direct connections to existing links across the M62 and M66 and attractive running and walking trails.

7.32 Routes should be designed to suitable dimensions to ensure spaces are proportionate to adjacent buildings and provide adequate amenity and landscape value. Proposed buildings adjacent to the boundary corridors should be of a suitable scale so not to dominate the character of the area.

7.33 The well-established and mature tree belts alongside the M62 and M66 are distinctive landscape features of the Character Area to be enhanced and extended with further structural landscape planting to cover the entirety of the motorway boundaries.



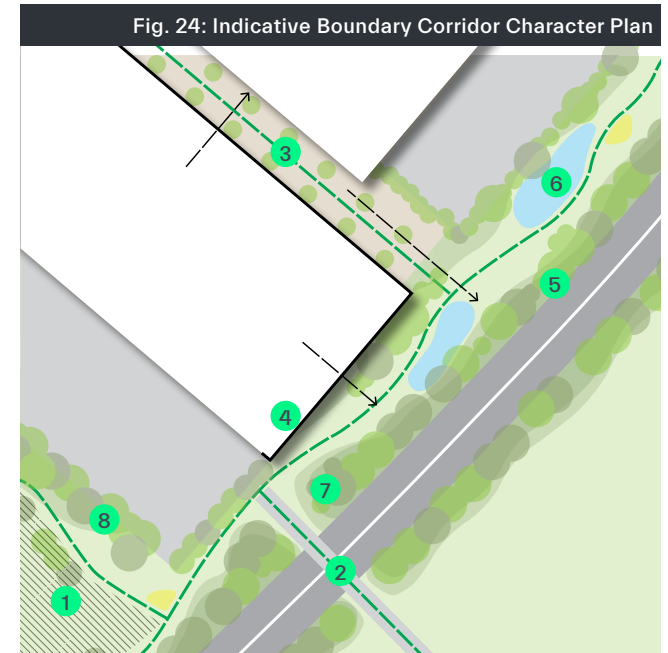
**The Boundary Corridor Character Area recognises the opportunity to create a high-quality, safe and enjoyable peripheral linear public open space that offers numerous benefits.**





## Boundary Corridors Key Design Principles

1. Building scale, use and form should be carefully considered where buildings sit in close proximity to adjacent proposed/existing residential uses in order to protect the amenity of residents.
2. Boundary Corridor will be largely free from road-based traffic and primarily take the form of pedestrian, cycle and Active Travel routes which connect into existing pedestrians/cycle routes across the M62 and M66 Motorways.
3. Due to the large format nature of employment-led developments such as Northern Gateway, where development allows, opportunities should be utilised to further improve permeability through the site with the introduction of additional pedestrian/cycle routes between development plots. These should offer direct routes and be designed to include suitable levels of natural surveillance, landscape and be accessible to all users. Built form to be set back from the M62 and M66 motorways to retain the existing mature boundary vegetation. Additional native tree belt planting to strengthen and diversify the existing tree belts where there has been weakening of the landscape structure.
4. Strengthened Boundary Corridor vegetation provides multiple noise, air quality and visual amenity mitigation benefits from the proximity of the M62 and M66 motorways.
5. Inclusion of naturalised and habitat friendly sustainable drainage systems to manage rainwater run-off from hard paved spaces and buildings.
6. The delivery of naturalised mitigation measures within Boundary Corridors also enhances the site's ecological habitats, connected wildlife movement corridors and Biodiversity Net Gain (BNG) contribution.
7. Boundary Corridors, within Northern Gateway, of linear green spaces with vegetation screening form separation distances transitioning between employment development and residential development.





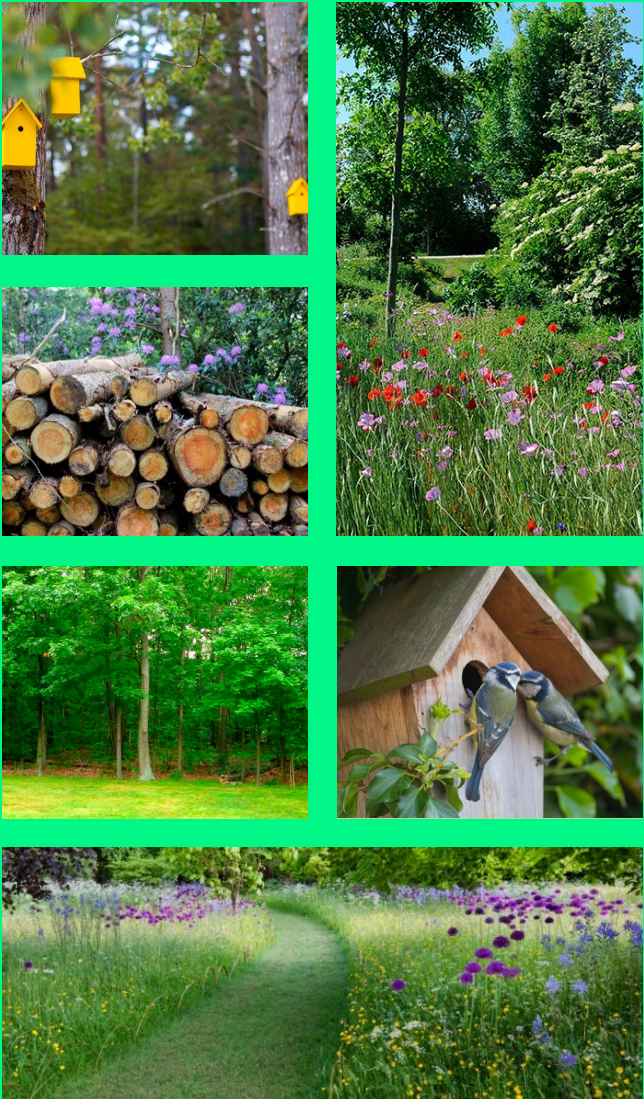
Built Form and Use



Streets and Movement



Landscape





## Character Area 6 – Castle Road

7.34 Land to the west of the site, off Castle Road, provides the opportunity to bring forward new high quality residential development within the site and set a high benchmark for future residential development within the wider Atom Valley. A new active travel route running east-west through this character area will provide the key connection point for new and existing communities to access the employment opportunities provided within the site and also the high-quality parkland provided within the Urban Valley. Existing recreation facilities will be retained and enhanced and contributions towards recreation and education provision made.



**Castle Road provides the opportunity to bring forward new high quality residential development within the site and set a high benchmark for future residential development.**

### Castle Road Key Design Principles

- Around 200 new homes, with the exact figure to be determined subject to infrastructure requirements.
- Provision of a range of house types, sizes and custom and self-build plots to create a vibrant and varied community. This should include affordable housing in line with the Councils' adopted policies and NPPF requirements and the density of development should be in line with PfE Policy JP-H4.
- Marker buildings should provide a gateway feature at the junction with Castle Road.
- Development blocks within the area should be orientated to promote east-west connectivity and ensure that streets and open spaces are active, overlooked and safe.
- Homes along the active travel route should create a strong frontage.
- Primary vehicular access to serve all residential parcels within this Character Area will be taken from Castle Road which will require a significant upgrade.
- Links to the existing Unsworth communities and into the wider employment site will be provided through the creation of a new active travel corridor through this Character Area. This route will be restricted to local bus services and will not provide connection for other vehicles between the employment and residential parts of the site.
- The movement network should encourage the use of public transport, walking and cycling ahead of the private car by providing direct linkages and improvements to the existing network
- Pedestrian and cycle routes through the site must offer logical and convenient connections to and between key destinations beyond the site boundary.
- Access to recreation provision must be considered when designing the street network.
- Existing road accessing Garlic must remain.
- The public realm throughout this character area should be high quality and help to foster a distinct identity which will also aid legibility.
- A variety of green spaces should be provided across the area ranging from private gardens, semi-private courtyard, pocket parks, informal areas of play to public open spaces. The incorporation of small-scale community orchards and allotments to aid health and well-being and enhance biodiversity will be encouraged.
- The design and integration of SuDs will be a key component in managing water across the area. Development should look to incorporate both soft and hard engineered features, which respond at various scales and include controls such as small-scale rain garden, swales, mini wetland and underground storage in addition to green roofs.

**Character  
Precedents** 



Built Form and Use



Streets and Movement



Landscape





# Sustainability

Greater Manchester's ambition is to become a carbon neutral city region by 2038.



08

Atom

.

Ville



## 08. Sustainability

Greater Manchester’s ambition is to become a carbon neutral city region by 2038. This ambition is supported by Bury and Rochdale Councils through respective declarations of climate emergencies. Achieving this goal will reduce the need for buildings and homes to be retrofitted in the future to achieve carbon neutral goals set by Greater Manchester and the UK.

8.1 Strategic Objective 7 of PfE “playing our part in ensuring that Greater Manchester is a more resilient and carbon neutral city region” includes two specific themes of relevance to this chapter which are to:

- Promote carbon neutrality of new development by 2028, and
- Improve energy efficiency and the generation of renewable and low carbon energy.

8.2 The scale and location of the site provides an opportunity to demonstrate a commitment to net zero through the implementation of the policies within PfE and the ambitions of Bury and Rochdale Councils. The delivery of a net zero and highly sustainable employment park will support green growth within the region and help attract innovative world-class industry.

8.3 This chapter focusses on the expectations and likely benefits from the site with respect to net zero, carbon, energy, resource efficiency and climate resilience which, collectively, would make a significant contribution to climate change mitigation and address the impacts of climate change<sup>5</sup>. The specific policies within PfE that are relevant to this section are:

- Policy JP-S1: Sustainable Development
- Policy JP-S2: Carbon and Energy
- Policy JP-S3: Heat and Energy Networks
- Policy JP-S4: Flood Risk and the Water Environment, and
- Policy JP-S6: Resource Efficiency.



**The scale and location of the site provides an opportunity to demonstrate a commitment to net zero through the implementation of the policies within PfE and the ambitions of Bury and Rochdale Councils.**

5. The policy expectations set out in this section do not apply to the South Heywood Masterplan Area which is subject to sustainability standards set out in the conditions of the outline planning permission(s).

## Carbon and Energy

**8.4** Development at the site must adhere to Policy JP-S2 to demonstrate how net zero has been achieved where practical and viable. The policy requires the following:

- From adoption (2024) – regulated operational carbon emissions: When the amount of carbon emissions associated with the building’s operational energy on an annual basis is zero or negative. A net zero carbon building is highly energy efficient and powered from on-site and / or off-site renewable energy sources, with any remaining carbon balance offset.
- From 2028 – the above plus all emissions ‘in construction’: When the amount of carbon emissions associated with a building’s product and construction stages up to practical completion is zero or negative, through the use of offsets. For clarity, construction carbon is defined as life stages A1-A5.

**8.5** The above definitions are adapted from the UKGBC Net Zero Carbon Buildings Framework.<sup>6</sup> The UKGBC Framework is due to be superseded during 2024 by the emerging UK Net Zero Carbon Building Standard<sup>7</sup> (UKNZCBS). Implications of this change will need to be addressed once the UKNZCBS is launched. However, initial recommendations of how the UKNZCBS can be addressed are set out within the relevant sections below.

6. UKGBC Net Zero Carbon Buildings Framework (2019)

7. UK Net Zero Carbon Building Standard

### Net Zero in Operation

**8.6** PfE Policy JP-S2 requires that regulated energy usage is minimised as far as possible on site in line with the following energy hierarchy:

- Minimise energy demand
- Maximise energy efficiency
- Use renewable energy
- Use low carbon energy, and
- Utilise other energy sources.

**8.7** Policy JP-S2 also requires that, for non-domestic buildings, BREEAM ‘Excellent’ standard (or equivalent) for the ‘Ene 01 – reduction of energy use and carbon emissions’ credit issue is achieved, rising to BREEAM ‘Outstanding’ equivalent from 2028. This requirement therefore forms the basis for how much energy and carbon emissions should be minimised directly on site.

**8.8** Residual emissions after reduction will then be offset where practical and viable via a fund which will be developed by GMCA, Bury and Rochdale Councils.

**8.9** From 2025, development should also calculate and minimise carbon emissions from unregulated emissions alongside regulated emissions. It is not expected that emissions associated with unregulated energy will be offset.

**8.10** It is expected that the energy strategy for the site will seek to be all-electric to ensure that energy demand is met through the increasingly decarbonising electricity grid. It is expected that heat pumps will be used to deliver the space heating and cooling demand.







**8.11** It is expected that on-site renewable energy will be maximised as far as possible at the site, prioritising rooftop and ground mounted solar photovoltaics, and building mounted micro-wind turbines where appropriate, practical, and viable.

### Net Zero in Construction

**8.12** From 2026, Policy JP-S2 requires (where practicable and viable) all new development to achieve zero carbon in construction in line with the UKGBC Framework (as defined in Paragraph 8.5 (b)), in addition to regulated energy. Construction carbon emissions should be minimised as far as possible before residual carbon is offset.

**8.13** Offsetting should be limited to buildings only, which aligns with the methodology within the UKGBC Framework and emerging UKNZCBS.

**8.14** Reductions in construction carbon should focus on material efficiency (e.g. 'use less') in the first instance, before exploring the use of low carbon and innovative materials.

### Carbon Offsetting

**8.15** In collaboration with the GMCA, Bury and Rochdale Councils are developing a Carbon Offset Fund to enable applicants to contribute to and achieve net zero.

**8.16** The fund is expected to be in place shortly and should be available for any relevant planning applications within the JPA1.1 site.

## Heat and Energy Networks

**8.17** The site has been identified as a Heat and Energy Network Opportunity Area. As such, it will be important to explore whether installation of a site-wide heat/energy network solution is practical, financially viable, and delivers the highest carbon savings over the course of its operation.

**8.18** Opportunities for using waste heat locally should be explored.

**8.19** Opportunities for smart energy networks should also be considered, including:

- Energy demand and generation balance
- Energy storage
- Microgrid(s), and
- Electric vehicle charging strategy (HGV, public, and private vehicles).
- If an applicant can demonstrate that an alternative energy strategy will deliver greater benefits, then this will be considered.

## Resource Efficiency

**8.20** Circular economy is a key part of Greater Manchester's ambition to be a carbon neutral and leading green city region by 2038, and Northern Gateway will have a role in supporting this.

**8.21** PfE Policy JP-S6 (Resource Efficiency) requires development to use sustainable design and construction techniques to reduce carbon emissions, adapt and future proof to the impact of climate change, reduce and recycle waste and minimise water use.

**8.22** It is expected that circular economy measures implemented in the development will include, but are not limited to:

- Material efficiency, minimising the quantity of materials used in construction
- Use of recycled materials and materials with a high recycled content
- Use of materials which can be repurposed or recycled at end of life
- Resource (energy and water) efficiency measures during construction and operation, and
- Reduction of waste generated during construction and operation, and diversion of residual waste from landfill.



## Climate Resilience

**8.23** Greater Manchester will experience changes to climate in the future, including:

- Increased annual temperatures
- Increased winter rainfall, and
- Decreased summer rainfall.

**8.24** Impacts associated with the above changes to climate and measures to mitigate them are set out within PfE Policy JP-S4 and must be adhered to.

**8.25** Development must demonstrate how climate adaptability and resilience measures have been incorporated through design measures which are appropriate for the JPA 1.1 site and its proposed use. Design measures expected to be implemented on site include:

- Reducing overheating risk through passive measures in the first instance, using active cooling where necessary.
- Reduction of surface water flooding through Sustainable Drainage Systems (SuDS) and positive interventions within the natural riparian environment working with natural processes, without significantly embodied carbon emissions.

- Incorporating green and blue infrastructure to mitigate overheating, flood risk, and contribute to biodiversity, whilst ensuring the design of built development also reduces wherever possible impacts on the surrounding natural environment.
- Minimising water use during operation through modern design techniques which promote measures for reducing water demand and incorporating rainwater or greywater harvesting, and
- Appropriate landscaping strategy which suits current and future climate scenarios and doesn't rely on mechanical irrigation.

## Environmental Accreditation

**8.26** As set out within Paragraph 8.8, development should achieve a minimum of BREEAM 'Excellent' standard (or equivalent) for the 'Ene 01 – reduction of energy use and carbon emissions' credit issue is achieved, rising to BREEAM 'Outstanding' equivalent from 2028.

**8.27** BREEAM provides a holistic approach to demonstrating sustainability, including categories in Energy, Water, Materials, Waste, and Pollution. The assessment process provides a robust approach to carbon and energy, resource efficiency, and climate resilience.

**8.28** Applicants are therefore encouraged to achieve full certification under BREEAM, to a rating of 'Excellent' as a minimum.

**8.29** Where feasible and viable, additional accreditation is encouraged to demonstrate the sustainability credentials of the site. Accreditation could include but is not limited to ActiveScore, FitWel, and WELL.

**8.30** Applicants are expected to maintain dialogue with the councils throughout the application process to ensure that the net zero and sustainability approaches are policy compliant and compatible with the ambitions of GM and Atom Valley.

**8.31** The measures set out may need to be reviewed following industry updates including Future Building Standard, UKNZCBS, and any further updates in national legislation and guidance.





# Phasing, Infrastructure and Delivery

This requires a coherent and coordinated approach to the provision of strategic infrastructure, through the direct delivery of on-site and off-site infrastructure, and through financial contributions.



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## 09. Phasing, Infrastructure and Delivery

The site is a large nationally significant location for new employment-led development. PfE anticipates the delivery of a total of around 1,200,000 sqm of industrial and warehousing space and around 1,200 homes. It is anticipated that the site has the potential to take approximately 20 years to be delivered, although the baseline assumption is that around 935,000 sqm of employment space will be delivered within the PfE plan period to 2039.

**9.1** This requires a coherent and coordinated approach to the provision of strategic infrastructure, through the direct delivery of on-site and off-site infrastructure, and through financial contributions. These contributions will ensure delivery of the site provides mitigation for the impacts of the development on the surrounding communities and the environment, as well as achieving the full aspirations of PfE.

**9.2** Planning permission has already been granted for up to 175,460 sqm of employment floorspace, 1,000 homes, a local centre, and a new primary school on the eastern part of the site at South Heywood, delivering an early phase of development. This chapter relates to the subsequent phasing and delivery of the remaining site.

**9.3** A comprehensive Infrastructure Phasing and Delivery Strategy (IPDS) for the site has been prepared in accordance with the PfE Policy JPA1.1 and JP-D1 and should be read alongside the NGDF SPD. The IPDS will remain a 'living document' and will be subject to regular updates and amendments outside of the NGDF SPD process in order

to be responsive to constraints and opportunities, and the availability of funding, as the site is brought forward.

**9.4** Planning applications within the site will be required to demonstrate how the proposed development would assist in the delivery of key infrastructure and not compromise or prejudice the comprehensive development of the site. The council(s) will need to be satisfied that development of individual parcels will not sterilise or frustrate delivery of other parts of the site and all parcels will need to be sufficiently coordinated with appropriate infrastructure delivered at the correct time to support comprehensive development. Planning applications will need to demonstrate alignment with the infrastructure requirements, delivery and phasing principles and programme set out in the IPDS. Piecemeal planning applications which fail to deliver or contribute towards the wider strategic infrastructure will be resisted.

**9.5** It is not considered practical to provide for the full infrastructure requirements across the site as part of its early development, particularly because of the scale of the

site, scheme viability and longevity of its expected delivery. Therefore, infrastructure delivery will be phased through the development cycle of the site, as parcels and phases are brought forward or in response to opportunities. Opportunities to secure external funding for key infrastructure will also be pursued to expedite the delivery of the opportunity.

**9.6** This includes the opportunity for the site to attract significant national and international investment, including a giga-scale opportunity. It is envisaged that the site will be promoted for such a giga opportunity and there is potential for this to be located within different parts of the site. If this opportunity does not come forward in the expected first phase of the site, then subsequent phases will still retain the potential. Implementing infrastructure throughout the site too early could constrain development parcels and investment opportunities of this scale.

**9.7** The IPDS will be kept under review and updated to respond to changing needs and circumstances over time.

## Phasing and Infrastructure

**9.8** Given the scale of the opportunity, full delivery of the site is expected to take place over a 20-year period. Development is expected to come forward in a series of phases alongside necessary infrastructure provision and a flexible approach is needed to be responsive to opportunities (which is one of the 12 key principles of the Northern Gateway Investment Zone summarised in Chapter 4 of this SPD).

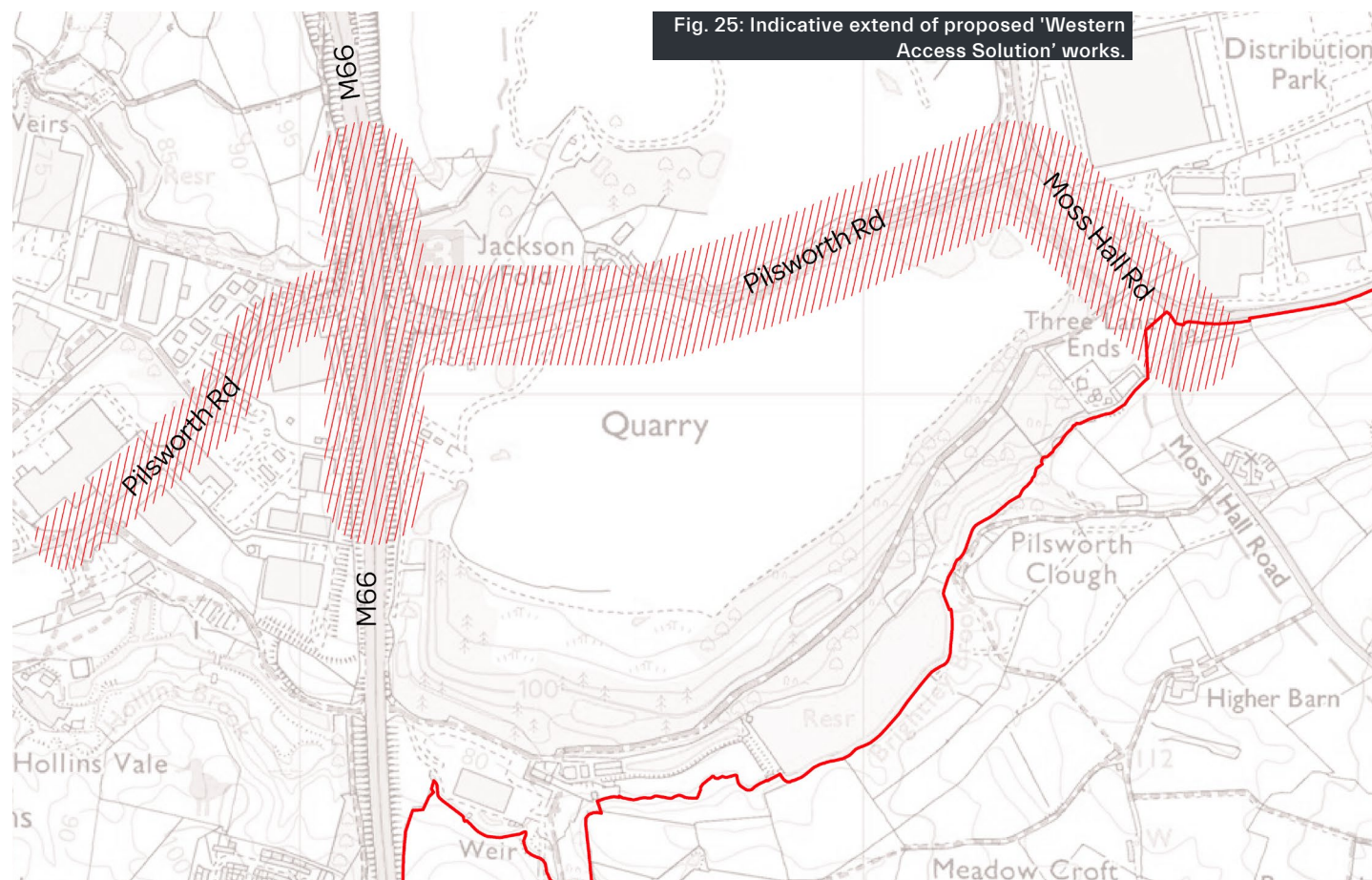
9.9 It is anticipated that there will be different phases across the lifespan of the development and over time the phasing may change and phases may emerge, depending upon the needs of future occupiers. The approach to phasing is not necessarily intended to be chronological, but rather reflects a natural and logical way the site will come forward given the existing infrastructure and the currently envisaged timing of delivery. This approach does not restrict the potential for latter phases to be accelerated where opportunities arise, or where infrastructure allows certain phases to be advanced earlier than currently envisaged.

**9.10** The overall phasing of development, including the quantum of floorspace capable of being delivered within each phase, will be influenced by the extent of available capacity within the surrounding road network (SRN) which will need to be managed with an integrated transport strategy which prioritises sustainable transport modes in accordance with the site transport vision set out in the Transport Framework Document.

## Initial Phase – Employment

**9.11** It is proposed that the logical and natural first phase of development (after the consented South Heywood development) will stem from the existing highway infrastructure that already exists in and around the site. The extent of the first phase and rate of delivery will be principally driven by market demand.

**9.12** Local highway upgrades are required to both the northern site entrance (Pilsworth Road/Moss Hall Road Junction (south)) and the existing spine road (Whittle Lane) running south through the site to allow initial phases of development in the northern part of the site which in effect will provide an extension to the existing built development and benefit from connection to existing infrastructure to commence.





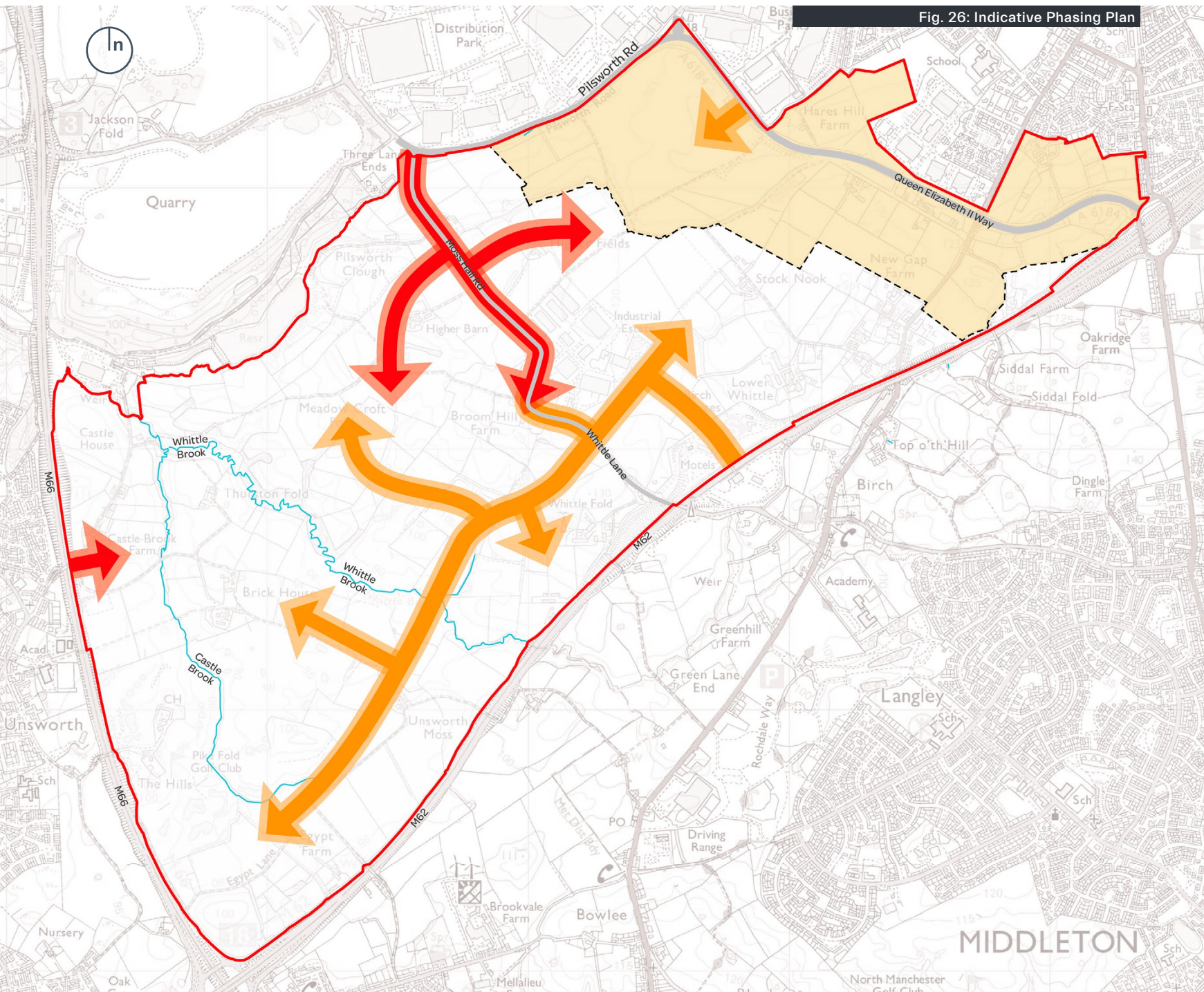


Fig. 26: Indicative Phasing Plan

- Allocation Boundary
- South Heywood Application Area
- Existing Road
- ➔ Initial Phases
- ➔ Later Phases

**9.13** A package of active travel and public transport improvements will be required to connect with nearby communities and town centres. Early planning applications within the site should deliver these interventions in a way that is proportionate and that establishes the principle of the site being accessed in line with the sustainable transport hierarchy, the Transport Framework Document and the IPDS.

**9.14** It is anticipated that the next phase of development opportunities will then be provided to the development plots to the east and west of the existing spine road, generally moving in a southerly direction from the Pilsworth Road/Moss Hall Road junction (south).

**9.15** To connect these initial phases of development with the local highway network and the strategic road network, improvements to Moss Hall Lane, Pilsworth Road (East and West of the M66) and Junction 3 of the M66 will be required. This infrastructure project is known as the 'Western Access solution' and will allow for increased highway capacity as well as early provision of public transport and active travel links.

## Initial Phase – Residential

**9.16** Residential development in the western part of the allocation is also expected to come forward in the early phases of the development. However, this will be independent of the employment part of the site, albeit with longer term requirements to link this site with the wider development as it progresses – specifically with regards to active travel and public transport.

**9.17** Primary access to serve the residential element will be taken from Castle Road and local highway upgrades at the junction between Castle Road and Pole Lane will be required to facilitate this access. A new active travel corridor from Castle Road and through the residential development will also be required as part of the residential phase, ensuring that land is safeguarded to provide connectivity to later phases of employment development outlined below.

## Later Phases

**9.18** As the employment development builds out from the north, further work will be undertaken to explore how later phases, and infrastructure, can be brought forward. Significant levels of investment in the SRN will be required and is likely to include a scheme of mitigation and improvement at Junction 3 of the M66 and Junction 19 of the M62. The exact requirements will be determined through joint working and detailed highways modelling with National Highways.

**9.19** Public transport access from Bury and Rochdale to the site will be improved and could include:

- Introduction of local bus services to/from key communities around the site, including, Bury, Heywood, Middleton, Rochdale, Radcliffe, Whitefield and Prestwich, as well as wider connections to the City Centre and Oldham.
- Active travel routes linking the site to surrounding communities and beyond, utilising existing crossing points across the strategic road network and potentially new crossing infrastructure.
- Tram-train on the Bury-Heywood-Rochdale line to the north of the site.
- A spur into the Northern Gateway site from the Bury-Heywood-Rochdale tram-train line.

**9.20** Later phases will also require internal highways infrastructure, including at least one bridge over Whittle Brook to connect the parcels with the internal spine road.

**9.21** Infrastructure in all phases must enable and link active travel and public transport connections throughout the whole site, including links through the residential component of the site although general traffic movement (including cars and HGVs) will not be permitted through this route..





## Flexibility and Adaptability

**9.22** Due to the scale and nature of the proposed uses, and the timescales within which it is anticipated to be delivered, the NGDF and associated Development Framework Plan needs to be robust as well as flexible. The NDGF provides suitable controls and principles to suitably guide future proposals and to manage design qualities, whilst being able to adapt to changing market conditions, context as well as occupier requirements. This includes factoring possible large investor requirements.

**9.23** The employment uses envisaged for the site could take the form of a larger number of smaller commercial units or a smaller number of large commercial units and will be driven by market demands and the needs of tenants. These will often require large, plateaued development plots for maximum flexibility to respond to the varied needs of occupiers and tenants.

**9.24** The scale of the site represents the largest new employment site in Greater Manchester and one of the few sites across the country with the potential to be able to accommodate a large investor 'Giga-scale' opportunity. Such opportunities are significant and would need special consideration with regard to setting, impacts and form of any such development. In order to accommodate such an opportunity, the Development Framework Plan has been designed to allow a number of adjacent development plots to be merged into a single development plot whilst not having a material impact on the site-wide design principles and the wider vision.

## Longer Term Development Potential

**9.25** PfE policy JPA1.1 requires existing recreation facilities within the site (including Unsworth Academy playing fields and Pike Fold Golf Course) to be retained and enhanced; or where necessary, for provision to be made for replacement facilities that are equivalent or better in terms of quantity and quality in a suitable location. In this context, the Development Framework Plan presented in this NGDF assumes that Pike Fold Golf Course and the adjacent playing fields will be retained.

**9.26** Notwithstanding this, there may be an opportunity in the future for development to come forward on the Golf Course and playing fields, subject to an alternative re-provision. As such, the Development Framework Plan has been designed to ensure that development doesn't inhibit future development of the golf course and/or playing fields should that part of the site come forward for development. This includes providing suitable potential points of access and through routes to ensure that any future development of the existing recreation facilities reads as part of a single comprehensive development.



**Fig. 27: Development Framework Plan showing potential development on the golf course**





## Site-Wide Infrastructure

9.27 Whilst the provision of new infrastructure is important, there may be existing infrastructure which is required to be diverted or amended as part of the proposed development. A coordinated approach to these works would be required to support delivery including:

- **Accessibility**
  - Active Travel
  - Public Transport
  - Road Network
- **Drainage and utilities**
  - The provision and diversion of water and drainage infrastructure
  - The provision and/or diversion of electricity/gas infrastructure
  - The provision or diversion of digital and telecommunications infrastructure
  - On site Energy
- **Strategic green infrastructure and biodiversity**
- **Community infrastructure**
- **Compensatory improvements to the environment quality and accessibility of land remaining in the Green Belt.**

## Delivery and Funding

9.28 A range of delivery and funding mechanisms will be required, including:

- **Developer contributions secured via Section 106 Agreements:** Developer contributions and/or delivery of infrastructure in lieu will be secured to assist in mitigating the impact of the development (or relevant phase of the development). In line with the CIL Regulations, contributions will be sought where they are necessary to make the development applied for acceptable in planning terms, and fairly and reasonably related in scale to the development.
  - **Public sector funding:** The scale of infrastructure required to realise the full potential of the site will require funding from the public sector. This funding is expected to help stimulate development and support the viable delivery of some of the key infrastructure required to realise delivery of the site.
- 9.29 An equalisation approach will be followed to ensure that the cost of infrastructure is shared across those benefiting from it. This approach seeks to avoid the costs falling disproportionately on early phases of development or being pushed back to later phases, which would be to the detriment of the comprehensive delivery of the site.

9.30 There will also be infrastructure components that are specific to the parcels that make up the site. Pre-application discussions with the council(s) will identify parcel-specific infrastructure needs, which will be additional to those site-wide strategic infrastructure components that all development should contribute to.

9.31 Bury and Rochdale Councils, as the Local Planning Authorities, have statutory powers that can enable and facilitate the delivery of major and comprehensive development opportunities. In order to ensure that the public benefits afforded by a comprehensive approach to development are fully realised, where necessary the councils will consider the use of their statutory powers (including compulsory purchase powers (CPO) and the over-riding of third-party rights) to deliver the allocated development on a comprehensive basis.



# Review

This NGDF SPD provides a clear vision and expectations for development and supporting infrastructure within the Northern Gateway JPA1.1 site.



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## 10. Review

This NGDF SPD provides a clear vision and expectations for development and supporting infrastructure within the Northern Gateway JPA1.1 site.

10.1 The implementation and delivery of the NGDF will take time but engagement with key infrastructure bodies will continue as sites and projects come forward.

10.2 If circumstances change and have implications for the guidance provided in the NGDF SPD, a review will be appropriate.

10.3 The scope and content of any review would be agreed between the councils and the updated NGDF SPD would be subject to a proportionate statutory and/or public/stakeholder consultation before being finalised. Thereafter, the updated Development Framework would be reported to a meeting of both Rochdale Borough Council and Bury Council Cabinets to seek approval.







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